

OFFICIALS

Dana Hennis, Chairman
Bryan Clark, Vice Chairman
Jim Main, Commissioner
Bobby Rossadillo, Commissioner

CITY OF KUNA
City Hall Council Chambers
751 W 4th Street, Kuna, ID 83634



Planning & Zoning Commission
REGULAR MEETING AGENDA
Tuesday, November 25, 2025, at 6:00 PM

For questions, please call Planning and Zoning at (208) 922-5546.
ALL AGENDA ITEMS ARE ACTION ITEMS UNLESS OTHERWISE NOTED.

1. CALL TO ORDER & ROLL CALL:

2. CONSENT AGENDA:

All items listed are routine and acted on with one (1) Motion by the Commission; there will be no separate discussion unless the Chairman, Commissioner, or Staff requests it be removed. Removed items will be placed under Business unless otherwise instructed.

- A. Regular Commission Meeting Minutes Dated October 28, 2025
- B. Cancelled Commission Meeting Minutes Dated November 12, 2025
- C. Decision and Reasoned Statement(s)
 - 1. Case No. 25-02-OA, Pole Signs
 - 2. Case No. 25-01-PUD, 25-03-ZC, 25-04-S, & 25-06-DR, Napa Vineyards

Potential Motion:

- *Motion to Approve Consent agenda.*
- *Motion to Approve Consent agenda with amendments (i.e., correction to previous meeting minutes, etc.)*

3. PUBLIC HEARINGS:

- A. Case No. 25-09-S & 25-29-DR, Indie Subdivision – Marina Lundy – Planner

Applicant requests Subdivision and Design Review approval for a 23-lot single family residential subdivision. The site is located near the intersection of W Ardell Rd. And S McClure Ln.

Potential Motions:

- *Motion to recommend approval/recommend denial of 25-09-S with Conditions as outlined in the staff report (and additional Conditions imposed by Commission, if applicable).*
- *Motion to approve/deny 25-29-DR, with Conditions as outlined in the staff report (and additional Conditions imposed by Commission, if applicable).*

4. BUSINESS ITEMS:

- A. Downtown Design Standards Amendment – Marina Lundy – Planner

The Kuna Development Services Department respectfully submits a proposed amendment to the "City of Kuna Downtown Design Standards."

5. *UPDATES & REPORTS:*

6. *ADJOURNMENT:*

OFFICIALS

Dana Hennis, Chairman
Bryan Clark, Vice Chairman
Jim Main, Commissioner
Bobby Rossadillo, Commissioner
Vacant, Commissioner

CITY OF KUNA
City Hall Council Chambers
751 W 4th Street, Kuna, ID 83634

Planning & Zoning Commission
REGULAR MEETING MINUTES
Tuesday, October 28, 2025, at 6:00 PM



For questions, please call Planning and Zoning at (208) 922-5546.
ALL AGENDA ITEMS ARE ACTION ITEMS UNLESS OTHERWISE NOTED.

I. CALL TO ORDER & ROLL CALL:

(Timestamp 00:00:23)

Chairman Dana Hennis Okay, we'll go ahead and call to order the regularly scheduled Planning & Zoning Commission meeting for Tuesday, October 28th, 2025. We'll go ahead and start with roll call.

Planning & Zoning Director Doug Hanson Chairman Dana Hennis.

Chairman Dana Hennis Present.

Planning & Zoning Director Doug Hanson Commissioner Bryan Clark.

Vice Chairman Bryan Clark Present.

Planning & Zoning Director Doug Hanson Commissioner Jim Main.

Commissioner Jim Main Present.

Planning & Zoning Director Doug Hanson Commissioner Bobby Rosadillo...

Chairman Dana Hennis Thank you.

COMMISSIONERS PRESENT

Chairman Dana Hennis - Present
Vice Chairman Bryan Clark - Present
Commissioner Jim Main - Present
Commissioner Bobby Rossadillo - Absent
Vacant Commissioner Seat - Vacant

CITY STAFF PRESENT

Doug Hanson, Planning & Zoning Director
Troy Behunin, Senior Planner
Marina Lundy, Planner
Matt Johnson, City Attorney

2. CONSENT AGENDA:

All items listed are routine and acted on with one (1) Motion by the Commission; there will be no separate discussion unless the Chairman, Commissioner, or Staff requests it be removed. Removed items will be placed under Business unless otherwise instructed.

(Timestamp 00:00:46)

A. Regular Commission Meeting Minutes Dated October 14, 2025

B. Decision and Reasoned Statement(s)

1. Case No. 25-08-S & 25-28-DR, Valor North Subdivision

Potential Motion:

- *Motion to Approve Consent agenda.*
- *Motion to Approve Consent agenda with amendments (i.e., correction to previous meeting minutes, etc.)*

(Timestamp 00:00:46)

Chairman Dana Hennis First up is the consent agenda.

Vice Chairman Bryan Clark Mr. Chairman, I move that we approve the consent agenda.

Commissioner Jim Main Second.

Chairman Dana Hennis All in favor?

All Commissioners Aye.

Chairman Dana Hennis Thank you.

(Timestamp 00:00:49)

Motion To: Approve The Consent Agenda

Motion By: Commissioner Bryan Clark

Motion Seconded By: Commissioner Jim Main

Further Discussion: None

Voting Aye: Commissioners Main, Clark, Hennis

Voting Nay: None

Absent: Commissioner Rosadillo

3-0-1

3. PUBLIC HEARINGS:

(Timestamp 00:00:57)

A. Case No. 25-01-PUD, 25-04-S, 25-03-ZC & 25-06-DR, Napa Vineyards – Troy Behunin – Senior Planner – *Continuation from October 14th, 2025.*

Applicant requests PUD, ZC, Preliminary Plat & Design Review approval for approx. 242 acres in Kuna City using the following zones P, R-6, R-20, C-1 & C-3. The Preliminary Plat proposes to subdivide the lands into 1,495 total residential, multiple common lots & common driveways, a School Site, Fire Station & a City Park in compliance with the approved Development Agreement. The site is in Sec. 12, Township 2 North, Range 1 West (APN numbers; S1312212400, R7135690180, S1312417410).

Potential Motions:

- *Motion to recommend approval/recommend denial of 25-01-PUD, 25-04-S, 25-03-ZC with Conditions as outlined in the staff report (and additional Conditions imposed by Commission, if applicable).*
- *Motion to approve/deny 25-06-DR, with Conditions as outlined in the staff report (and additional Conditions imposed by Commission, if applicable).*

(Timestamp 00:00:57)

Chairman Dana Hennis Okay, and so first up on our items tonight for public hearings is case #25-01-PUD, #25-04-S, #25-03-ZC and #25-06-DR for Napa Vineyards, which is a continuation from October 14th meeting, Troy.

Senior Planner Troy Behunin Thank you very much for that lead-in, appreciate that. Commissioners, for the record, Troy Behunin, Development Services Department, Kuna, Idaho. This is a continuation of the Napa Vineyards Planned Unit Development, also known as the PUD that was continued from the October 14th hearing. Tonight, we're just going to pick up where we left off at the end of the meeting, it was discussed that tonight's meeting would be opened for the introduction of three critical agency comments, which have all been submitted, and they're in the packet available for the public and also for the commission. That would be for ACHD, ITD, and also Public Works. At the end of the meeting, there was a motion for the continuation to allow the public to give testimony on those three items and limit it to those three items, but that it would be opened up to the public again, and then the applicant would be able to rebut anything and answer any of the questions that the Commission has for anything, not just for those projects, for those three items, but for anything. And staff will also be here in case you have questions for staff. And with that, I will turn it back over to you.

Chairman Dana Hennis Thank you. Is there anything we need to address before we open back up for a public hearing or testimony? I don't think so. I don't think so. And Actually, we never closed public testimony, did we? So we just need to continue.

Senior Planner Troy Behunin That's correct.

Chairman Dana Hennis So is there any other folks here in the audience tonight that would like to testify on this application that did not sign up? Okay, and seeing none, I will go ahead and ask the applicant if

they would like to come up with a rebuttal. *Okay. I'm sorry, sir. Did you want to testify on the application? Come on up. We'll have you sign in. Come on up here, please. Just put your name and address so we can have it in the record.* Is there anybody else that just came in that would like to testify on the application for the Napa Vineyards tonight? *Just you? Yeah. Okay. And you can go ahead and take the podium. At the base of the microphone, there's a light. Can you just press that until it turns green?* And then if you would, state your name and address for the record.

Michael Anderson Michael Anderson, 8400 South Updale Avenue, Meridian.

Chairman Dana Hennis *And you go ahead and speak.*

Michael Anderson To which application?

Chairman Dana Hennis For this is the Napa Vineyards application, which is the residential and commercial addition that along Meridian Road between Hubbard and Columbia.

Michael Anderson Okay. I would like to know what research has been done to the facilities that would be needed to support this size of a facility would be unneeded has been done to this point. If the adequate research hasn't been done, why are we going forward with this?

Chairman Dana Hennis Do you have a particular area that you're asking for the research of?

Michael Anderson Water, power, that would be my main concerns. School, of course. But water and power and sewer would be the main concerns for me being a current resident of the area.

Chairman Dana Hennis Okay. And the applicant will hopefully answer some of these questions for you. Is there anything else?

Michael Anderson I'm looking for specifics.

Chairman Dana Hennis Okay.

Michael Anderson Not generalities.

Chairman Dana Hennis Okay.

Michael Anderson What do we have here that would support what they're proposing? Or, what would it take to support that if the current facilities don't support that?

Chairman Dana Hennis Correct, and that's what they would need to they would need to elaborate on.

Michael Anderson Absolutely. Okay, thank you.

Chairman Dana Hennis Thank you. Okay, I'll ask once again, is there anybody else in the audience that would like to testify on this application that has not signed up yet? Okay, and seeing none, I'll go ahead and close -- Well, I guess we'll have the rebuttal first. So, applicant.

Stephanie Hopkins, KM Engineering Thank you, Mr. Chair, Members of Commission. Stephanie Hopkins of KM Engineering. Our address is 5725 North Discovery Way. I will talk about the utilities and some of the services that are provided per Mr. Anderson's question or comments. Public Works is one of the staff reports that we were waiting on at the last hearing, and since that hearing, they have submitted comments that include a variety of conditions and requirements that would be imposed on this project.

They do that for every project. So, they usually will review and assess a project and determine what the city's current capacity levels are and require improvements to sewer water and utilities that they manage. So, one of the things here is that the Mason Creek trunk line is going to be extended as part of this project. It's actually going to be a huge improvement to the Kuna sewer capacity. And as part of this, they are requiring or requesting an easement that would be brought through this project as well to help with that extension. Water is addressed as well, and I'm trying to find specific conditions with that. There is a City well that's approved, and this project would have to contribute to that City well to be able to increase the capacity within Kuna. And we discussed this in the last hearing, but this project is required to pay impact fees for each lot that is proposed with building permits. So those fees go to fund these different entities that serve the City of Kuna. So that includes water, it will include sewer, parks, police, all of the different services and entities that are impacted by development. Another thing to note is that anything that we design will have to go through the City Engineer for approval before we're actually able to construct or even get approval on construction plans. So there's a pretty rigorous review process that this project will have to go through.

And it's a large project. So it's 242 acres. There are going to be, I believe, 11 phases of development. So the first phase is about 66 single-family residential homes. It also includes the fire station lot and a commercial lot that's in the northwest corner. All of that first phase will be reviewed by the City Engineer, as well as other reviewing agencies within that have jurisdiction instead of Kuna. So department -- IDEQ, there would be, sorry, all these acronyms, Idaho Transportation Department, ACHD -- Idaho Power will even review our plans, and they'll have to determine if they have the capacity and ability to serve. Idaho Power and other servicers within the Treasure Valley are constantly looking at development that's going on, and they're upsizing facilities to accommodate growth in the Treasure Valley. So, there will be adequate services and if there aren't adequate services, this development would be required to upsize whatever it is that is being impacted. So that is, I think, hopefully addresses that question and concern. I'll let Sonia maybe cover the traffic stuff because that was a huge topic of discussion the last time around and we have a presentation if you want to see anything that would help visualize.

Chairman Dana Hennis Thank you.

Stephanie Hopkins, KM Engineering Thank you.

Sonia Daleiden, Kittleson & Associates Thank you, Stephanie. Chair, Commissioners, Sonia Daleiden with Kittleson & Associates. The address is 101 South Capital Boulevard, Suite 600 in Boise. Although there wasn't public testimony on it tonight, I know we did leave the hearing open waiting on the detailed staff reports from the Ada County Highway District and Idaho Transportation Department. So, I just wanted to add that those have both been received, as Troy mentioned, and the applicant is in full agreement with all of the conditions listed by ACHD and ITD related to the transportation system. I can -- happy to answer any detailed questions about any of the specific transportation mitigations, but there's an extensive list, I think 50 in the ACHD report and then several in the ITD report as it relates to Highway 69 related to new roadway construction, roadway widening, turn lane additions, new traffic signals at certain triggers. And yeah, the applicant's in full agreement with all those conditions and we'll be implementing those if they receive approval to move forward.

Chairman Dana Hennis Okay, thank you.

Sonia Daleiden, Kittleson & Associates Happy to answer any questions.

Chairman Dana Hennis Any questions from the commission at this time?

Commissioner Jim Main Sure. For our traffic engineer, one question I have is the intersection of Linder Road and Columbia. The thing that I'm a little concerned with is ACHD said typically the applicant would be required to improve this intersection. But since there's no available right-of-way, therefore no mitigation is required. So how can it be something that would be recommended by both the traffic study and ACHD but not required just because there's no right-of-way? Wouldn't there be something that would indicate that right of way needed to be acquired for that?

Sonia Daleiden, Kittleson & Associates Chair, Commissioner, thank you for the question. Yeah, it's a great question. So, the way it is written in the ACHD staff report is actually a pretty typical condition in that type of situation. ACHD and the developer, they are not able to acquire right-of-way from a private property owner that this applicant does not have control over. So, this applicant does not have any legal mechanism to go about acquiring right-of-way from a private property owner to implement a project if it's not available. What they are doing, and the condition that ACHD has put in this case, is they have required the developer to do follow-up study at that intersection if right-of-way becomes available in the future. And this development is also paying traffic impact fees to ACHD. Those traffic impact fees go towards a general pool right now that ACHD can use to either purchase right of way in the future or implement projects that are on its Capital Improvement Plan... Linder, Columbia is an intersection that does have a future Capital Improvement plan project so although ACHD can't force this developer to go out and purchase right of way or acquire right away from another property owner they are paying into a future pool of money that ACHD can use to implement improvements there in the future.

Commissioner Jim Main How will they implement improvements there in the future if they can't acquire the right-of-way?

Sonia Daleiden, Kittleson & Associates Well, so ACHD is a government entity. They do have the ability to, if property in that area does redevelop, to require whoever is doing that redevelopment to donate right-of-way. As a government entity, they also have the power to go out with eminent domain and purchase right-of-way from the property owners there if they see the public need for improvements in the future. So that's how ACHD would go ahead and do an improvement there in the future. However, they just can't require another private entity such as this developer to go and get right-of-way from another private entity because we don't have a private entity doesn't have the government authority of eminent domain.

Commissioner Jim Main Thank you.

Sonia Daleiden, Kittleson & Associates Welcome.

Chairman Dana Hennis Any further questions?

Vice Chairman Bryan Clark So, I guess the only question I have at this point is based on these conditions...it does look like a lot of this is actually developer, not just funded, but developer construction, correct?

Sonia Daleiden, Kittleson & Associates Chair, Commission, that is absolutely correct. I mean, anything in the ACHD staff report that says is a condition of approval, that would be both funded and constructed by the development. And then if you look at the ITD staff report, they do require turn-lane funding and construction at 69 and Columbia, 69 and Deer Flat, and excuse me, 69 and Hubbard, and then a potential

new signal at Mason Creek in 69 when the subsequent phase of the development moves forward. There are some general funds that are required through impact fees and proportional share contributions that then would be used by the agencies for construction. But everything else written in the staff reports would be developer funded and constructed. That's correct.

Chairman Dana Hennis Anything further?

Commissioner Jim Main No.

Sonia Daleiden, Kittleson & Associates Thank you.

Chairman Dana Hennis With that, I'll go ahead and close the public testimony at 6:17, and that'll bring up our deliberation.

Vice Chairman Bryan Clark It's a big one.

Chairman Dana Hennis Yeah, yeah, that's an understatement. Not understatement.

Vice Chairman Bryan Clark Not to understate things, but I think that's really the big thing that we're up against here. It does, as a big project, does potentially accelerate the need for a lot of these improvements. Which is potentially a benefit, including the trunk line, sewer trunk line, as well as a lot of these permanent road improvements. But that does come with a cost. Our schools are, again, we can't overstate that enough -- Our schools are currently inundated, even though this comes with the school site, it doesn't come with the funding to build a building on the school site. So now, just as a reminder, or just as an afterthought on the school issue, there is stipulation on the contract that the school gets first opportunity for denial. I can't remember what the exact phrasing is, but they get first crack at the land.

Chairman Dana Hennis Right, first right of refusal.

Vice Chairman Bryan Clark First right of refusal, thank you. So that's another piece of the puzzle.

Chairman Dana Hennis Yeah, I mean, we can't get a lot of these road improvements without a development funding a lot of these. And it's kind of a double-edged sword. And we've talked about this over the last few years, that the need arises after -- actually the construction arises after the need a lot of the time. And that's how it is in highway districts all throughout the nation. I mean, they're never building roads ahead of time. They just can't, even though it makes sense. But to get these improvements that are badly needed in a lot of these intersections, there needs to be somebody that does it. And although this project would give a lot of that traffic mitigation that is needed in that area, it's at the cost of a lot of extra people and an impact going in. But I really, I still struggle with the whole school thing. I appreciate the fact that they're given a school site. But again, we've got several locations throughout our city here that have had the donations by the developers, but they can't get the funds to build the schools. And we can have as many sites as we can tolerate, but if we can't build a school on it, it does not help. And if we had 200 or 300 homes coming in, it'd be different than 1,500. And I know that the school representative last time had stated that the numbers to the amount of people, the amount of people coming in, it used to be 1.2 children per household, or per door, basically, and it's now down to what they estimated, 0.5 per door. That's better than what it used to be, but it's still 700 more kids at the school at best, or you know, at best and none of them near that area have that capacity. The nearest one is Silver Trail and they're already at overcapacity. And so I've still got a real hard time saying that this large of an area, even though it has so

many benefits to the City, the traffic, the Mason trunk line extension, so many of this infrastructure build outs, I mean they're going to have to bring -- improve power down that way. They're going to have to improve the pressurized irrigation, et cetera, for that area.

Vice Chairman Bryan Clark Which is taxed.

Chairman Dana Hennis Right. And we've got commercial coming in that'll help the tax base. It's just the schools are what killed me on this one. I don't know.

Commissioner Jim Main Question for the developers rep. You've indicated that there's a school site on the property and that the school district has the first right of refusal. Is this a dedicated school site if in fact the school district does refuse the property? I know in past projects, Mr. Eck has partnered with charter schools to build facilities. Is that what they would be looking at here also?

Senior Planner Troy Behunin Just for the record, you'd have to reopen the public hearing, I believe, in order to ask the development. the developer the question, but I can answer it for you.

Commissioner Jim Main Could you answer that for me, Troy?

Senior Planner Troy Behunin So just to make sure that I understand your question, you know, what happens if the school district doesn't take that? What happens to that property? Is that essentially the question?

Commissioner Jim Main Correct.

Senior Planner Troy Behunin So the way that the development agreement that was approved by city council reads is that they have the first right of refusal. He has other options that are available to him. If that does happen, a charter school would be the fallback position. At least that's the testimony, and that's what he's conveyed to staff. And yes, it does seem like a pattern being established, but where everybody is concerned with funding schools and things like that, charter schools have shown that they don't have the same funding, financial issues. But that would be the that would be the case.

Vice Chairman Bryan Clark So as a follow on to this question, Jim, if you don't mind. I respect Mr. Eck and everything he's done. He's done a lot of great work. But does this amount to a handshake or is there any guarantee that, that is reserved for a school of some sort in the future? Because again, he's I like what he does and I think that's great, but I just, are we guaranteed the school site?

Chairman Dana Hennis I think maybe to back end on that is because it's stated in the presentations that it's a school or park. So if -- I think what we're asking is if the school district refuses that right to purchase it or to take it, then does it go to a park or another school? I mean, what's the guarantee on that?

Planning & Zoning Director Doug Hanson For the record, Doug Hanson, Planning & Zoning Director, the only real guarantee for that if the school district were not to take the site is that it would have to be something to count towards qualified open space. So, it could be a school or it could be a park. It could never be developed as homes.

Vice Chairman Bryan Clark Right.

Chairman Dana Hennis Okay. Thank you.

Vice Chairman Bryan Clark Again, he has a good track record. I don't doubt that would be his goal, but it just has to be said that there's a possibility that it'll -- I mean, you know, it's still for public benefit at the very least.

Chairman Dana Hennis Correct.

Vice Chairman Bryan Clark So.

Chairman Dana Hennis Yeah, if it were only so easy. I mean, they're willing to do all the traffic mitigation requirements, and that's extensive, with what has been presented by both ACHD and everything. We all know these need to be done. I don't think there's much of a way we'd get it without it.

Vice Chairman Bryan Clark I mean, the benefit of a larger development like this is with the sheer number of required improvements. Now, the way those conditions are written, and staff, please correct me if I'm wrong, the only milestone I saw in there was, I believe, the 200th lot for a couple of traffic signal improvements. Everything else looks like it's 'no, you're going to construct this without a timeline.'

Chairman Dana Hennis There was one 300 lot.

Vice Chairman Bryan Clark There was one 300 lot.

Chairman Dana Hennis But everything else is that, yeah, that's as far up as it went.

Vice Chairman Bryan Clark So based on my reading, these are, so staff, how is this going to be worked out? Is this a, you will have this in before you get your first building permit or is this kind of a before you close your last building permit, these things need to be done?

Senior Planner Troy Behunin For the record, Troy Behunin. So, there are milestones. There are certain things that they have to accomplish prior to the final plat getting signed by the City and by ACHD. Remember, ACHD is the signature on the final plat, every final plat. So, they've got to be in whatever they say has to be in by the 201st lot, 301st lot, they have to be in.

Vice Chairman Bryan Clark Correct.

Senior Planner Troy Behunin In, built, tested, approved, accepted.

Chairman Dana Hennis Right, I think what Brian's asking is there were several conditions in there that really didn't state at to what level it had to be done. So, do we assume that they were done in the, need to be done in the beginning? Or, because I only saw the 220th lot and the 300th lot, and the others just kind of were in between. So, they weren't real clear to me.

Vice Chairman Bryan Clark Yeah, so starting at condition 5, dedicate right of way at Columbia Road, it doesn't appear to be tied to a, unless there's a statement in here, all other conditions shall be...

Senior Planner Troy Behunin So those individual or those in between without a lot assigned to them, is going to be controlled by whatever phase of that project it fronts. So if that requirement is for phase five, but it is in between the 221st and the 321st, if it's in between there, it's going to be ruled by the frontage. And that is the standing ACHD condition. So, you have to -- I mean, there are some floating conditions, yes, but they also have their standard procedure where you've got to improve everything along your frontage and what is deemed to be a part of it. So if they have to extend a turn lane or if they have to

extend an accel lane or a decel lane ahead of other improvements, they've got to do that for that phase. They have to become whole with ACHD and the City the entire way, whether there's a milestone or not.

Chairman Dana Hennis Okay. And I think we just want this not just to be clear to us, but also to the people that are here listening to this.

Senior Planner Troy Behunin And anybody who has questions, they're certainly welcome to contact us, me, or they can reach out to ACHD and get a clearer understanding. They are the traffic authority and the traffic experts.

Chairman Dana Hennis It's basically, if we're starting up here, they're not going to mandate stuff down here. They're going to mandate the stuff around the phase that you're in the middle of, in other words.

Senior Planner Troy Behunin Correct. I mean, that's the general rule, is that you improve everything along your frontage, but there are exceptions where you have to improve, improve things beyond for excel lanes, decel lanes in order to accommodate safety and other things that are needed. Just like the city, I mean, it doesn't matter where you start, you have to have sewer, right? All of that comes off site. So, it's kind of one of those things.

Chairman Dana Hennis Thank you.

Vice Chairman Bryan Clark All right, so that's fair. So, I mean, for instance, constructive right turn center turn lane on Columbia Avenue at Kay. So, the map that touches Columbia and Kay is going to be dedicating that right away and doing those improvement plans.

Chairman Dana Hennis Correct.

Vice Chairman Bryan Clark So, okay.

Chairman Dana Hennis Yeah, that's that was an extensive report, so it was a little confusing. You get a few in, you know what you're kind of what they're saying. You get towards the bottom and you lose track of them.

Vice Chairman Bryan Clark Yeah, I'm not sure how I feel about this one. I mean, it's really is a good looking project. From the aesthetics, I mean, from the design review standpoint, we're getting a good amount of open space and greenery. And I feel like this is going to be overall a good thing. The future multifamily, which again will trigger another TIS, I believe, if I recall correctly.

Chairman Dana Hennis I would imagine it's got to.

Vice Chairman Bryan Clark Sorry. Quick question just to ask. Will the future multifamily also trigger another traffic impact analysis?

Senior Planner Troy Behunin I...

Vice Chairman Bryan Clark I can't remember.

Senior Planner Troy Behunin I don't know that one for sure. It likely will. However, the school will all by itself, whether it's a charter school or a public school.

Vice Chairman Bryan Clark Right, because there's a public safety aspect to that.

Senior Planner Troy Behunin They're both public schools, but whether it's a charter school or whether it's KSD.

Vice Chairman Bryan Clark Right.

Chairman Dana Hennis Okay.

Vice Chairman Bryan Clark Okay so -- and honestly, we're hurting for multifamily in the area. We really are, so...

Commissioner Jim Main Well, the way, or the things I see here, you know, we're looking at commercial, which would increase the tax base. We're looking at a school site. We're looking at getting infrastructure put in. A lot for a fire station. And the project, I think, is over a 10-year period from the schedule that I saw. And the fact that the multifamily in the front would trigger another TIS as well as the school. You know, it's big. And personally, I'm not, I don't care much for big things, but looking at what this will accomplish as far as infrastructure, as far as commercial, and as far as improvements on Highway 69 and the other intersections. There's a lot that there's a lot that they're bringing to the table.

Vice Chairman Bryan Clark I'm going to ask a really awkward open-ended question. And one of the big reasons that we're going to a PUD as opposed to just sitting within the existing zoning is the fact that they are asking for these, for the 3,600 square foot lots.

Chairman Dana Hennis Yeah.

Vice Chairman Bryan Clark Okay. So, I don't want to lose sight of that. I don't have a problem with that product. Actually, my first home was on a 3,500 square foot lot, and the benefit of that was it was my first home, and it was an opportunity to get into the market. So, there are definitely benefits to having a smaller product, especially in our area where we have a lot of bigger houses. But how do we feel about it up against the R-1, or I'm sorry, the rural residential on the east? We do have a really good buffer. We have the lateral that's going through there. But I just want to put it on, I just want to make sure that we don't overlook it as part of our discussion. We do have some green space, but that entire 70-foot or the lateral right-of-way cannot be improved beyond gravel is my understanding because of the easement and the maintenance requirements. So that being said, I just, I don't want to lose sight of that as part of our discussion here.

Chairman Dana Hennis Yeah, I mean, that's a good thing to bring up. I mean, I think there's a lot of buffer there. And we've been told by several realtors and people outside of just developers that that's the product that is being looked for because there's a couple of different age glasses that just don't want to have the responsibility of having a yard or a large yard to take care of. It's a product that people are looking for, not just as a starter home, but just as sometimes a retirement home, and just that they don't want to take care of all the stuff that the rest of us do and fight over. So, I mean, I agree the product is one that is actively looked for, which is obviously what you want in development. And I think, we've got most of those properties all set to the other side. Then you have the easement, or you have the whole canal, then you have the easement, then you have the landscaping, then you have the homes, and you know they're not going to be real tall anyways. So, I'm not sure that I'm too worried about that.

Usually, yes, we require a lot more transition there, but I think we inevitably have got that transition between the canal and the easement and then the landscaping that's required.

Because I have a feeling a lot of that's going to be landscaped and blocked or whatever you want to call it. There's going to be a lot of site obstruction there with the landscaping because it appears that quite a few trees and everything are being planned.

Vice Chairman Bryan Clark And there is a landscape buffer up to the lateral.

Chairman Dana Hennis Yeah, I mean, I echo Jim's comments. I think this gives us a lot of what we need in the area for improvements. I'm still worried about the cost of the schools. I know the developer has made some real concessions to go with charter schools, which have been well received by the public. It just depends on what Kuna School District is able to do with this one. Obviously, we'd love to have the district be able to provide a school there. But at least there's a backup plan that the kids will still have a place to go, hopefully. So hopefully soon. But it will take a while to fill this up. And they aren't starting in this phase of the multi-family first anyways. It's going to be in the...

Vice Chairman Bryan Clark Single family, commercial first.

Chairman Dana Hennis Commercial, commercial first.

Vice Chairman Bryan Clark Yep. Okay.

Chairman Dana Hennis It's a double-edged sword on this one. You know, it's... I came into this whole thought as not wanting this development, to be quite honest. Even though this has been around for 15 years, 18 years? 18 years. Started in 2008.

Vice Chairman Bryan Clark 2008.

Chairman Dana Hennis And so, you know, back then it was a great development to come in because there was nothing like it in the area.

Vice Chairman Bryan Clark There was also nothing around it.

Chairman Dana Hennis Exactly. And we had far less houses and developments at that point that were contributing to the school problem. Back then we didn't have as much of the school problem. And so, that got me a little bit worried this coming in and I didn't want to see it come in, but in seeing what they presented to us, I think it does have some good attributes and hopefully we can solve the school problem in the next 10 years with that one, you know, I'm still at a loss.

Vice Chairman Bryan Clark Yeah and that's that is unfortunately where I kind of struggle is it's hard to you always want to be hopeful, but at the same time it's is it responsible to rely on hope? Is the question. So yeah. Yeah.

Commissioner Jim Main Even with the school site, a school just doesn't pop up. And in order for the district to build a facility, they have to run a bond. They have to go through the design process. They have to go through the construction process. So they're looking at a minimum of three to four years to get into a facility.

Chairman Dana Hennis Correct.

Commissioner Jim Main So based on that, By the time they could get into a facility, half of this development would be up.

Chairman Dana Hennis Well, I'm not quite sure if half of it would be up.

Commissioner Jim Main Yeah, they're looking at, we're looking at a 10-year process for these homes. So the need for the school would be there before the school was there.

Chairman Dana Hennis But we do have three other charter schools that are going to be on line before then.

Commissioner Jim Main Right.

Chairman Dana Hennis So, I mean, one starts this fall. The other two should be within the next two years, realistically.

Vice Chairman Bryan Clark And that's going to take a lot of pressure off of the K through 7, K through 6, K through 8. But really, it's the it's the overall impact to our high schools. The good news is, I mean, Swan Falls at least half built. But even then, I mean, it is definitely impacted. So I'm really of two minds on this project. And that's obviously the struggle with all our conversation up here. I think we're all of two minds. Yeah, I don't know what the alarm is.

Chairman Dana Hennis Yeah. Hopefully it's not something important. All right. Well, I mean, yeah, if there's no other concerns or comments then...

Vice Chairman Bryan Clark Yeah. Okay.

[Brief Silence]

Chairman Dana Hennis This many people will increase traffic. But we got traffic. There's a ton of traffic mitigation that's going to occur with this, by this developer. We've got the infrastructure for electrical. If not, they're going to build it. Same with water. They're already saying they're going to build the sewer, the sewer treatment plants down the road. So it's not like it has to go far. We've got a school site, we've got a park, we've got a fire station. The only thing we're missing is a substation for the police. We've got commercial coming in. I worry about the future multi-family. That's a bunch of extra that we haven't even talked about. But that has to come before us down the road. And they may opt to go to commercial on that if something plays out differently.

Vice Chairman Bryan Clark So the other aspect of this is none of this except for the 3600 is outside of the existing zoning. So, to some extent this project is coming whether we approve this one or another one down the line. I guess that's kind of one of the other aspects of this. It's already zoned in that direction.

Chairman Dana Hennis It's already zoned. It's already got development agreement. It's already, I mean, that's the whole thing. This was ready to go and approved in 2008. The only thing that's different is what's happened in the last 17 years. And that's been my concern. That's like I said, is I was not against it back then. But there's a lot that's happened since then that is a concern. I mean, we hear it from our people in the city, the other citizens. Why are we developing so much more? We've already got traffic. We've got this. We don't have the schools for it. You know, I want to listen to those people around me. This is why I'm on the committee and the commission and just to try to make a difference and voice some of these concerns and yet, in being here for a while, I understand that you've got to have some development to pay for some of those things to do and occur. But I also don't want everybody else to think we're failing them by approving this kind of thing.

I mean, it's already approved, really. We're just opening the can of worms again. But we do get a lot out of this one. I just, I'm worried about the schools. Yep, still. This isn't gone anywhere. I keep saying this and it's just, you know, I'm trying to make sense as to what, really what makes sense on this one. For half the size, it would be different. Well, third the size. It's a lot of homes.

Vice Chairman Bryan Clark Yep.

Chairman Dana Hennis And I don't -- and yeah it's like it's already zoned for all of this so it's not like we're changing anything or can stop what was zoned way back when.

Vice Chairman Bryan Clark Right.

Chairman Dana Hennis So it's inevitable to put it simply. But what we might lose is some of the amenities, the benefits, the traffic mitigation they're willing to pay for.

Vice Chairman Bryan Clark Right.

Chairman Dana Hennis Because somebody else can come in here and do something not quite so nice and not provide quite as much.

Vice Chairman Bryan Clark The other aspect of that is one of the benefits of a PUD is, I mean, we kind of have a good idea of what's coming now, as opposed to, like you said, if this got split up in the future into multiple developments, each one of them is going to be a different can of worms on its own. And I mean, this way we know what we're getting, we know what the future looks like. And more importantly, the future is being planned for on a large scale.

Chairman Dana Hennis And to explain that a little bit, if it gets split up, we wouldn't have some of the traffic mitigation that's being provided, really. Not on this much of a scale.

Vice Chairman Bryan Clark Yeah, I mean, the DIF fees wouldn't change. I mean, whether one developer pays it or multiple, I mean, potentially in the future, those DIF fees are going to go up in the future. So, I mean, But, I mean, that's definitely not a... What I'm getting at, though, is the overall impact. It's going to be the same. It's really just a...It would definitely stretch it out over time, but the...

Chairman Dana Hennis Maybe. It's not a guarantee.

Vice Chairman Bryan Clark Right. That is true. But the other aspect is with a development this large, it's going to be stretched out over time unless they get 7 builders knocking on their door ready to build tomorrow and are all ready to go for a phase at a time. So... and that's a possibility. You could have seven different builders walk in and drop sticks on the same day.

Chairman Dana Hennis Yeah, you've still got to get the infrastructure in.

Vice Chairman Bryan Clark Correct. Correct. But to my -- the point I was trying to make is with something that's large, it's going to take a little more time to actually get going, potentially. So the impacts aren't realized on day one.

Chairman Dana Hennis No.

Vice Chairman Bryan Clark And hopefully by the time -- okay, I'm using the word hope again. The way it's currently planned, the infrastructure will be in place by the time it's needed, based on the conditions. So.

[Brief Silence]

Chairman Dana Hennis That future multifamily still, that worries me a little bit.

Vice Chairman Bryan Clark So what is the primary concern with the multifamily, just the density? Because if I recall correctly, it's a, what's the number on those?

Chairman Dana Hennis R-20.

Vice Chairman Bryan Clark R-20s, right. Yeah, we have some existing R-20 in the city. I believe pretty much most of our apartments are sitting in the R-12 to R-20 range. The nice thing about that is they are going to come back to us. So approved, are they currently approved at R-20? It's currently zoned R-20.

Chairman Dana Hennis Yeah, that's, yeah, there's no change in the zoning. That was given 17 years ago.

Vice Chairman Bryan Clark Right.

Chairman Dana Hennis And we can't do anything to change that.

Vice Chairman Bryan Clark Right, but they're going to come back for design review at the very least.

Chairman Dana Hennis Correct. Yeah, I mean...

Vice Chairman Bryan Clark And final plat.

Chairman Dana Hennis Yeah. And it just depends. Yeah, we can look at it, but we can't. And I think that's just kind of what needs to be understood is we can't change that right at this point, but it does worry me. And who knows, that could be another 20 years. Don't know. Would hope, but...there's that darn word again.

Vice Chairman Bryan Clark Yeah, it's a scary word.

Chairman Dana Hennis Well, this is a recommendation to the city council. They ultimately will hear more public testimony. They will evaluate it from their own accords. It's just whether we think, given the zoning, the layout, the design review, et cetera, I guess we do have a design review component to this one.

Vice Chairman Bryan Clark Yes.

Commissioner Jim Main Yeah.

Chairman Dana Hennis But the PUD and everything is going to be, they have final. So there will be another hearing and information on this. So, I guess if there's a motion.

Vice Chairman Bryan Clark You looking at me, Jim, or am I looking at you, Jim?

Commissioner Jim Main I'm looking at you.

Vice Chairman Bryan Clark Oh, man. All right.

Chairman Dana Hennis It's the only good thing about being here. Ok

00:51:34 **Vice Chairman Bryan Clark** Okay, so here's where the rubber meets the road, All right Mr. Chairman, I move to recommend approval of #25-01-PUD, #25-04-S, #25-03-ZC with the conditions as outlined in the staff report.

Commissioner Jim Main I'll second that.

Chairman Dana Hennis Thank you. All in favor?

Commissioners Aye.

Chairman Dana Hennis Any opposed? I will stand as opposed.

(Timestamp 00:51:34)

Motion To: Recommend Approval of Case #25-01-PUD, #25-04-S, #25-03-ZC With Conditions

Motion By: Commissioner Bryan Clark

Motion Seconded By: Commissioner Jim Main

Further Discussion: None

Voting Aye: Commissioners Main, Clark

Voting Nay: Commissioner Hennis

Absent: Commissioner Rosadillo

2-1-1

Vice Chairman Bryan Clark Mr. Chairman, I move that we approve #25-06-DR with conditions as outlined in the staff report.

Commissioner Jim Main Second.

Chairman Dana Hennis Thank you. All in favor?

Commissioners Aye.

Chairman Dana Hennis Any opposed? I will stand opposed on that as well. Thank you. Thank you for all your attention on that discussion.

(Timestamp 00:52:08)

Motion To: Approve Case #25-06-DR With Conditions

Motion By: Commissioner Bryan Clark

Motion Seconded By: Commissioner Jim Main

Further Discussion: None

Voting Aye: Commissioners Main, Clark

Voting Nay: Commissioner Hennis

Absent: Commissioner Rosadillo

2-1-1

B. Case No. 25-03-S & 25-04-DR Lerida Subdivision – Marina Lundy – Planner

The applicant requests approval to Subdivide an approx. 21.69-acre parcel at 819 N Meridian Road (APN S1324142300) into 93 single family residential lots and 14 common lots.

Staff requests this Case to be tabled to a date uncertain.

Potential Motions:

- *Motion to table Case 25-03-S & 25-04-DR to a date uncertain.*

(Timestamp 00:52:38)

Chairman Dana Hennis I will go ahead and move to the next item, which is case #25-03-S and #25-04-DR for the Lorita subdivision, I believe.

Planning & Zoning Director Doug Hanson For the record, Doug Hanson, Planning & Zoning Director, City of Kuna, 751 West 4th Street. Staff is requesting that this item be tabled to a date uncertain as we are still working out some sewer serviceability components for the project and are waiting on final public work comments.

Chairman Dana Hennis Thank you.

Vice Chairman Bryan Clark Mr. Chairman, I move that we table case #25-03-S and #25-04-DR to a date uncertain.

Commissioner Jim Main Second.

Chairman Dana Hennis Thank you.

Chairman Dana Hennis All in favor?

All Commissioners Aye.

Chairman Dana Hennis Any opposed? No? Thank you.

(Timestamp 00:53:13)

Motion To: Table Case #25-03-S and #25-04-DR To A Date Uncertain

Motion By: Commissioner Bryan Clark

Motion Seconded By: Commissioner Jim Main

Further Discussion: None

Voting Aye: Commissioners Main, Clark, Hennis

Voting Nay: None

Absent: Commissioner Rosadillo

3-0-1

C. Case No. 25-02-OA, Pole Signs – Doug Hanson – Planning & Zoning Director

The City of Kuna Planning and Zoning Department requests a zoning text amendment application. The text changes include modifications to the Development Regulations Code for pole signs.

Potential Motions:

- *Motion to recommend approval/recommend denial of 25-02-OA with Conditions as outlined in the staff report (and additional Conditions imposed by Commission, if applicable).*

(Timestamp 00:53:30)

Chairman Dana Hennis Last item up on the agenda in the public hearings portion is case #25-02-OA, Pole Designs, Doug.

Planning & Zoning Director Doug Hanson Good evening, Commissioners. For the record, Doug Hanson, Planning & Zoning Director, 751 West 4th Street, Kuna, Idaho. The City of Kuna Planning & Zoning Department requests a zoning text amendment application for modifications to the development regulations code for pole signs. Staff believes that the proposed changes will better serve the intent of the sign code to protect property values and create an attractive community while encouraging creative, readable, and uncluttered and well-maintained signage. Special Use Permit approval would be required for the construction of all pole signs, and the zoning districts and locations where pole signs can be constructed will be limited. With the proposed code change, pole signs will only be allowed within the north-south corridor alignment of State Highway 69 Meridian Road corridor, located within C-2 area commercial and C-3 service commercial zones, limiting the impact of these signs have to a heavy traffic, predominantly commercial developments, and preserving the neighborhood commercial character of other arterial corridors throughout the city with the neighborhood commercial uses and significant residential development. And with that, I will stand by for any questions.

Chairman Dana Hennis Thank you. Any questions?

Commissioner Jim Main No.

Chairman Dana Hennis I have one simple question. What's the definition, the city definition of a pole sign?

Planning & Zoning Director Doug Hanson So the city definition of a pole sign would be a sign erected on a pole-like structure with the predominance of the square footage being placed on top at or above 30 feet in height. And in this case, that standard would change to just be, it could be below 30 feet in height as well. We took away that limitation. There used to be pull signs to 30 feet and then pull signs beyond. This would just recognize like one standard of a pull sign.

Chairman Dana Hennis So, the permanent. Not all the little ones they poked in the ground.

Planning & Zoning Director Doug Hanson For the record, Doug Hanson, Planning & Zoning staff, or director, we would consider that a pylon sign or a temporary banner sign.

Chairman Dana Hennis Okay, thank you. Any other questions for staff? No. Thank you. So with this, we'll go ahead and open up the public testimony for this application at 6:56. And I don't have anybody

currently signed to testify on this application for the poll sign amendment. Is there anybody in the audience that would like to? And seeing none, I will go ahead and then close the public testimony at 6:56. And that'll bring up our deliberation. I think this one's pretty straightforward. It's just kind of rewriting and clarifying.

Commissioner Jim Main Right, and just clarifying the code. I don't have any issue with it.

Chairman Dana Hennis Yeah, I didn't either. I just kind of wanted to make sure that all these little temporary ones are still classified as temporary. With that, I stand for a motion then.

Commissioner Jim Main Mr. Chairman, I move to recommend approval of #25-02-OA with conditions as outlined in the staff report.

Vice Chairman Bryan Clark Seconded.

Chairman Dana Hennis Thank you. All in favor?

All Commissioners Aye.

Chairman Dana Hennis Any opposed? Thank you.

(Timestamp 00:56:54)

Motion To: Recommend Approval Of Case #25-02-OA With Conditions

Motion By: Commissioner Jim Main

Motion Seconded By: Commissioner Bryan Clark

Further Discussion: None

Voting Aye: Commissioners Main, Clark, Hennis

Voting Nay: None

Absent: Commissioner Rosadillo

3-0-1

4. BUSINESS ITEMS:

5. UPDATES & REPORTS:

(Timestamp 00:57:16)

Chairman Dana Hennis With that, that completes the list of agenda items that I have in front of me tonight. Is there any business items or updates from staff?

Planning & Zoning Director Doug Hanson That is it for the evening.

Chairman Dana Hennis Thank you.

6. ADJOURNMENT:

(Timestamp 00:57:29)

Commissioner Jim Main Mr. Chairman, I move that we adjourn.

Vice Chairman Bryan Clark Seconded.

Chairman Dana Hennis Thank you. All in favor.

All Commissioners Aye.

Chairman Dana Hennis Thank you. Thank you all for coming.

(Timestamp 00:57:29)

Motion To: Approve The Consent Agenda

Motion By: Commissioner Jim Main

Motion Seconded By: Commissioner Bryan Clark

Further Discussion: None

Voting Aye: Commissioners Main, Clark, Hennis

Voting Nay: None

Absent: Commissioner Rosadillo

3-0-1

Dana Hennis, Chairman

ATTEST:

Doug Hanson, Director

Minutes prepared by Garrett Michaelson, Deputy City Clerk.



CITY OF KUNA

751 W 4th Street • Kuna, ID 83634
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P&Z Commission Public Hearing Sign-In Sheet
 October 28, 2025

Case Nos.: 25-01-PUD, 25-04-S, 25-03-ZC and 25-06-DR.

Case Name: Napa Vineyards PUD Subdivision

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Planning & Zoning Commission Public Hearing Sign-In Sheet
 October 28, 2025

Case No.: 25-02-OA (Ordinance Amendment)

Case Name: POLE SIGNS

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OFFICIALS

Dana Hennis, Chairman
Bryan Clark, Vice Chairman
Vacant, Commissioner
Jim Main, Commissioner
Bobby Rossadillo, Commissioner

CITY OF KUNA
City Hall Council Chambers
751 W 4th Street, Kuna, ID 83634



Planning & Zoning Commission
REGULAR MEETING MINUTES

Wednesday, November 12, 2025, at 6:00 PM

For questions, please call Planning and Zoning at (208) 922-5546.
ALL AGENDA ITEMS ARE ACTION ITEMS UNLESS OTHERWISE NOTED.

1. CALL TO ORDER & ROLL CALL:

2. CONSENT AGENDA:

All items listed are routine and acted on with one (1) Motion by the Commission; there will be no separate discussion unless the Chairman, Commissioner, or Staff requests it be removed. Removed items will be placed under Business unless otherwise instructed.

1. Regular Commission Meeting Minutes Dated October 28, 2025
2. Decision and Reasoned Statement(s)
 1. Case No. 25-02-OA, Pole Signs

Potential Motion:

- *Motion to Approve Consent agenda.*
- *Motion to Approve Consent agenda with amendments (i.e., correction to previous meeting minutes, etc.)*

3. PRESENTATIONS:

- A. Ada County Highway District Development Services Presentation

4. PUBLIC HEARINGS:

5. BUSINESS ITEMS:

- A. Downtown Design Standards Amendment - Marina Lundy, Planner

The Kuna Development Services Department respectfully submits a proposed amendment to the "City of Kuna Downtown Design Standards."

6. UPDATES & REPORTS:

7. ADJOURNMENT:

CLERK'S NOTE: A QUORUM OF COMMISSIONERS NOT BEING PRESENT, NO BUSINESS WAS AGREED TO.
THE PLANNING & ZONING COMMISSION STANDS ADJOURNED UNTIL THE 25TH OF NOVEMBER, 2025.

Dana Hennis, Chairman

ATTEST:

Doug Hanson, Director

Minutes prepared by Garrett Michaelson, Deputy City Clerk.

**BEFORE THE PLANNING & ZONING COMMISSION
CITY OF KUNA, ADA COUNTY, IDAHO**

IN THE MATTER OF THE APPLICATION OF)	Case No.: 25-02-OA (Ordinance
)	Amendment) Pole Signs
THE CITY OF KUNA)	
)	
<i>Ordinance Amendment to Title 5,</i>)	
<i>Development Regulations of Kuna City</i>)	DECISION AND REASONED
<i>Code.</i>)	STATEMENT.

**SECTION 1
INTRODUCTION AND PROCEDURAL BACKGROUND**

THESE MATTERS came before the Planning & Zoning Commission of the City of Kuna (the “Commission”) for Public Hearing on October 28, 2025, the Commission entered a recommendation on one (1) application. Thereafter, this Decision and Reasoned Statement was prepared and presented to the Commission on November 25, 2025, for formal adoption of the Commission recommendation.

**SECTION 2
RECCOMENDATION**

The Commission, having considered the record and listened to the arguments and presentations at the hearing, DOES HEREBY ORDER AND DECIDE:

The Ordinance Amendment Application (Case No. 25-02-OA) is hereby *Recommended Approval*.

**SECTION 3
RECORD OF EXHIBITS AND WITNESSES**

I. List of Exhibits.

- DEQ
- KSD
- DRAFT Signage Ordinance REDLINES
- NARRATIVE
- PROPOSED CHANGES
- AFFIDAVIT OF PUBLICATION PZ 10.28.2025 POLE SIGNS
- AGENCY TRANSMITTAL

II. Public Hearing Witnesses.

Witness Testimony: Those who testified at the Commission October 28, 2025, hearing are as follows, to-wit:

Pole Signs

- a. City Staff:
Doug Hanson, Planning & Zoning Director
- b. Appearing in Favor:
- c. Appearing Neutral:
- d. Appearing in Opposition:

**SECTION 4
REASONED STATEMENT**

I. Relevant Criteria and Standards Considered

- a. Relevant City Ordinance Provisions.
 - 1. Title 5 Kuna City Code, Development Regulations
- b. Relevant Statutory Provisions.
 - 1. Chapter 65 of Title 67, Idaho Code
 - i. I.C. § 67-6511 – Zoning Ordinance
- c. Pertinent Constitutional Provisions

II. Factual Findings

- a. Procedural Findings:

Agency Comments Request	August 12, 2025.
Idaho Press Newspaper Published Hearing Notice Commission	Published on October 10, 2025.
Commission Public Hearing	Held on October 28, 2025.
Commission Written Decision and Reasoned Statement Entered	Entered on November 25, 2025.

- b. Relevant Contested Facts.
 - 1. No testimony was presented in opposition to the project, therefore no contested facts were entered into the record.
- c. Commission’s Factual Findings on Relevant Contested Fact.

1. None.

III. Rationale for the Decision Based Upon Facts and Relevant Criteria and Standards Considered.

It is the rationale of the Commission that the proposed changes regulating pole signs better met the intent of the sign code to protect property values and create an attractive community, while encouraging creative, readable, uncluttered and well-maintained signage.

**SECTION 5
REQUIRED NOTICES TO APPLICANT AND AFFECTED PARTIES**

I.C. 6735(2)(b) Any applicant or affected person seeking judicial review of compliance with the provisions of this section must first seek reconsideration of the final decision within fourteen (14) days. Such written request must identify specific deficiencies in the decision for which reconsideration is sought. Upon reconsideration, the decision may be affirmed, reversed or modified after compliance with applicable procedural standards. A written decision shall be provided to the applicant or affected person within sixty (60) days of receipt of the request for reconsideration or the request is deemed denied. A decision shall not be deemed final for purposes of judicial review unless the process required in this subsection has been followed. The twenty-eight (28) day time frame for seeking judicial review is tolled until the date of the written decision regarding reconsideration or the expiration of the sixty (60) day reconsideration period, whichever occurs first.

BY ACTION OF THE CITY COMMISSION of the City of Kuna at its regular meeting held on the 25th day of October 2025.

Dana Hennis, Chairman

**BEFORE THE PLANNING & ZONING COMMISSION OF THE
CITY OF KUNA, ADA COUNTY, IDAHO**

IN THE MATTER OF THE APPLICATIONS OF) **Case Nos.:** 25-01-PUD (Planned Unit
) Development), 25-03-ZC (Rezone),
NE KUNA FARM, LLC, AND) 25-04-S (Preliminary Plat), and
OPEN DOOR RENTALS, LLC) 25-06-DR (Design Review)
)
Related to the development of real property)
located South of Columbia Rd. and North of) **DECISION AND REASONED**
Hubbard Rd., West of Meridian Rd. (Parcel) **STATEMENT FOR NAPA**
Nos. S1312212400, R7135690180,) **VINEYARDS.**
S1312417410.).)

**SECTION 1
INTRODUCTION AND PROCEDURAL BACKGROUND**

THESE MATTERS came before the Planning & Zoning Commission of the City of Kuna (“the Commission”) for Public Hearing on October 14, 2025, and was continued until October 28, 2025, where the Commission entered recommendations and decisions on the application. Thereafter, this Decision and Reasoned Statement was prepared and presented to the Commission on November 25, 2025, for formal adoption of the Commission’s recommendation and decisions.

**SECTION 2
RECOMMENDATION AND DECISION**

The Commission, having considered the record and listened to the arguments and presentations at the hearing, DOES HEREBY RECOMMEND AND DECIDE:

The P.U.D. Application (Case No. 25-01-PUD) is hereby *Recommended Approval*.

The Rezone Application (Case No. 25-03-ZC) is hereby *Recommended Approval*.

The Preliminary Plat Application (Case No. 25-04-S) is hereby *Recommended Approval*.

The Design Review Application (Case No. 25-06-DR) is hereby *Approved*.

**SECTION 3
RECORD OF EXHIBITS AND WITNESSES**

I. List of Exhibits.

- AFFIDAVITS OF LEGAL INTEREST
- AMENDED DEVELOPMENT AGREEMENT
- APPLICATION ANNEXATION AND ZONING
- COMMITMENT TO PROPERTY POSTING

Napa Vineyards Subdivision

- DESIGN REVIEW APPLICATION
- ELEVATIONS
- GEO-TECH REPORT
- GRADING & DRAINAGE PLAN
- LANDSCAPE PLAN
- LEGAL DESCRIPTIONS
- NARRATIVE
- NATURAL FEATURES MAP
- NEIGHBORHOOD MEETING CERTIFICATION
- PRE PLAT APPLICATION
- PRELIM CONSTRUCTION DAWINGS
- PRELIMINARY PLAT
- PLANNED UNIT DEVELOPMENT APPLICATION
- PLANNED UNIT DEVELOPMENT SITE PLAN
- SUBDIVISION NAME RESERVATION
- TRAFFIC IMPACT STUDY (TIS)
- VICINITY MAP
- WARRANTY DEED
- VAPPLICANT PRESENTATION TO PZC
- APPLICANT PRESENTATION PZ 11.12.25
- IP PUBLICATION PZ 11.12.2025
- PROOF OF LEGAL NOTICE MAILER PZ 10.25.2025
- PROOF OF PROPERTY POSTING PZ 10.17.2025
- IP PUBLICATION CC 1.10.2025
- PROOF OF LEGAL NOTICE MAILER CC 1.10.2025
- PROOF OF PROPERTY POSTING CC 1.3.2025
- IP PUBLICATION PZ 6.6.2025
- PROOF OF LEGAL NOTICE MAILER PZ 6.3.2025
- PROOF OF PROPERTY POSTING PZ 6.13.2025
- AGENCY TRANSMITTAL 7.17.2024
- ACHD
- ADA COUNTY DSD
- BKID
- BPBC UPDATED
- CDHD
- COMPASS
- DEQ
- DEQ UPDATED
- ITD
- ITD UPDATED
- KRFD
- KSD
- KUNA PARKS & REC DEPT.
- NMID
- NMID DUAL LETTER
- PUBLIC WORKS

- CITIZEN Email – Yore 8.19.2025
- PZ CASE PACKET 10.14.2025
- STAFF REPORT PZ 10.14.2025
- PZ DECISION & REASONED STATEMENT 11.25.25
- PZ MINUTES 10.14.2025
- PZ MINUTES 10.28.2025
- SIGN UP SHEET PZ 10.14.2025
- SIGN UP SHEET PZ 10.28.2025

II. Public Hearing Witnesses.

A. Witness Testimony: Those who testified at the Commission October 14, 2025, hearing are as follows, to-wit:

1. City Staff:
Troy Behunin, Senior Planner, Kuna, DSD
2. Appearing for the Applicant:
Stephanie Hopkins, KM Engineering, 5725 N Discovery Wy., Boise, ID
Sonja Daleiden, Kittelson & Assos. 101 S Capital Blvd. Boise, ID
3. Other Testimony:
Tim Jensen, KSD, 711 E Porter St., Kuna, ID
Melissa Dean, 2574 N Hose Gulch Ave., Kuna, ID
Seamus Hodges, 8105 S Chugiak Pl. Kuna ID
Tim Eck, 6357 Buena Vista, Star, ID

B. Witness Testimony: Those who testified at the Commission October 28, 2025, hearing are as follows, to-wit:

1. City Staff:
Troy Behunin, Senior Planner, Kuna, DSD
2. Appearing for the Applicant:
Stephanie Hopkins, KM Engineering, 5725 N Discovery Wy., Boise, ID
Sonja Daleiden, Kittelson & Assos. 101 S Capital Blvd., Boise, ID
3. Other Testimony:
Michael Anderson, 8400 S Updale Ave., Meridian, ID

SECTION 4 REASONED STATEMENT

I. Relevant Criteria and Standards Considered

a. Relevant City Ordinance Provisions.

1. Kuna City Code 1-14-3

Napa Vineyards Subdivision

2. Title 5 Kuna City Code, Development Regulations
 3. Title 5 Kuna City Code, Chapter 8 Zoning Code, Part 9, Design Review
 4. Title 5 Kuna City Code, Chapter 9 Subdivision Code
 5. Title 5 Kuna City Code, Chapter 8 Zoning Code, Part 1, Rezone Process
- b. Relevant Statutory Provisions.
1. Chapter 13 of Title 50, Idaho Code.
 2. Chapter 65 of Title 67, Idaho Code
- c. Pertinent Constitutional Provisions

II. Factual Findings

- a. General Factual Findings.
1. Statement of Fact

Parcel Number(s):	S1312212400, R7135690180, S1312417410.
Future Land Use Map Designation:	Mixed-Use and Medium Residential Density
Existing Land Use:	Residential, Farm Fields
Current Zoning:	P, R-6, R-20, C-1 & C-3 (Kuna City)
Proposed Zoning:	P, R-6, R-20, C-1 & C-3
Development Area:	242.03 acres
Adjacent Zoning Districts:	North: R-20, R-6 & RR; East: C-1, A, (City) RR (County); South: R-6 & RR; West: R-4 & RR.
Adjacent Street(s) Existing & Proposed:	North: W Columbia Rd.; East: Meridian Rd.; South: Hubbard Rd.; West: Kay Ave..
Internal Street(s) Existing & Proposed:	Proposed: W Greensprings, W Chappellet, W Skyracer, W Cabernet, W Frantesca, W Bianco, W Consention, W Mondavi, W Frogs Leap, W Paloetti, W Rocca, W Red Wine, W Opus, W Etude, W Schramsberg, W Mason Creek Road, W Ice Wine, W Franc, W Petite Syrah, W Napa Valley, W Semillion, W Temprenillo, W French Oak, W Canelli, W Riesling, W Grenache, W Hubbard Road, S Carignane, S Chenin Blanc, A Kay Avenue, S Overture, S Dolce, S Amarone, S Gewurz, S Malbec, S Zinfandel, S Red Ruby, S Rockledge, S Pinot Gris, S Malbec, S Monastrell, S Viogier.
Adjacent Bike/Pedestrian Facilities:	None

Adjacent Parks:	Providing a School Lot, City Park, internal open spaces and paths, and paths along canal.
Land Dedication Requirements:	N/A

2. Procedural Findings:

Neighborhood Meeting	Held on October 4, 2024, and January 7, 2025. Two (2) Attendees were present.
Pre-Application Meeting	Held on July 11, 2024
Agency Comments Request	July 15, 2025.
300 FT Legal Mailer Notice	Sent on August 7, 2025.
Idaho Press Newspaper Published Hearing Notice	Published on August 14, 2025.
Site Posting	Posted on June 13, 2025.
Planning and Zoning Public Hearing	Held on June 24, 2025.
Planning and Zoning Commission Written Recommendation and Decision Entered	Sent to P&Z August 12, 2025.

b. Relevant Contested Facts at the October 14, and 28, 2025, Public Hearings:

1. Members of the Public testified;

- a. The Columbia Road & Hubbard Roads corridors are congested.
- b. Concern about timing of roadway improvements for the roads and general utilities.
- c. Concern about studies and research being conducted for the project.
- d. Concern about the classroom sizes.
- e. The development would add additional congestion, lights and noise to the area.

2. A Traffic Impact Study (TIS) which was accepted by ACHD outlines Site Specific Conditions of Approval in order to proceed. These Conditions would mitigate traffic concerns and improve the flow and congestion for the area and maintains acceptable levels of Service (LOS). In ACHD's report in Section A-3 (Specific Conditions of Approval, it lists improvement milestones for required roadway improvements which need to be in place before ACHD will sign the plat for the 220th lot and the 300th lot. There are other phased improvements needed in between all phases in order for ACHD to sign each plat.

c. Commissions Factual Findings on Relevant Contested Fact.

1. From evidence and testimony presented, the Commission determined that the subject road corridors and intersection are or will perform at an acceptable level of service and the proposed improvements will positively impact service levels.

III. Rationale for the Decision Based Upon Facts and Relevant Criteria and Standards Considered.

It is the rationale of the Commission that the PUD, rezone, preliminary plat and design review applications are in compliance with Idaho Code, Kuna City Code and the Comprehensive plan.

IV. Conditions of Approval.

1. Buffers, curb, gutter and sidewalk (attached and detached) shall be installed in accordance with *KCC 5-10-13-B-1-b*.
2. Developer/Owner/Applicant shall work with Ada County Highway District and the City of Kuna to complete all required traffic improvements to the surrounding roadways and intersections as detailed in the Ada County Highway District staff report.
3. Developer/Owner/Applicant shall install a sign at the terminus of the proposed stub street stating; “this road will continue in the future”. Developer/Owner/Applicant shall obtain proper language from Ada County Highway District.
4. Developer shall place a Contractors Sign at the entry of the subdivision.
5. Developer/Owner/Applicant shall measure all front building setbacks from back of sidewalk on all internal local roads.
6. Developer/Owner/Applicant shall ensure the proper easement widths on all lots in accordance with *KCC 5-8-1117-D-6*.
7. It is the responsibility of the Developer to ensure any anticipated buildings fit any given buildable lot in accordance with *KCC 5-8-503*.
8. Fencing within and around the site shall comply with *KCC 5-8-905* (unless specifically approved otherwise and permitted).
9. All required landscaping shall be permanently maintained in a healthy growing condition. The property Owner shall remove and replace unhealthy or dead plant material within 3 days or as the planting season permits as required to meet the standards of these requirements. Maintenance and planting in public Rights-of-Way shall be with approval from ACHD.
10. Landscaping shall not be placed within ten (10) feet of any meter pits, pressurized irrigation valves and/or ACHD underground facilities and must honor all vision triangles.
11. The Landscape Plan and Preliminary Plat as submitted will be considered binding site plans as amended and/or approved.
12. All signage within/for the project shall comply with Kuna City Code and shall be approved through the applicable sign approval process listed in *KCC 5-8-901-B*.
13. If any revisions are made, the Applicant shall provide the Planning and Zoning Staff with a revised copy of the Preliminary Plat. Any revisions of the Plat are subject to Administrative Determination to rule if the revision is substantial.
14. Developer/Owner/Applicant is hereby notified that this project is subject to Design Review inspection fees. Required inspections (post construction), are to verify building and landscaping compliance prior to requesting signature on the final plat.
15. Installation of service facilities shall comply with the requirements of the public utility or irrigation district providing the services. All utilities shall be installed underground, see *KCC 5-9-402-B-22*.
16. Compliance with *I.C. §31-3805* is required. Delivery of water shall not be impeded by any construction on site. Compliance with the requirements of the Boise Project Board of Control is required.
17. When required, submit a petition to the City (as necessary, confirmed with the City Engineer) consenting to the pooling of irrigation surface water rights for delivery purposes and request to annex the irrigation surface water rights appurtenant to the property over to the Kuna Municipal Pressure Irrigation System of the City (KMIS).
18. Connection to City Services (Sewer, Water, Pressurized Irrigation) is required. The Applicant shall conform to all corresponding City of Kuna Master Plans.
19. The Developer/Owner/Applicant shall be required to participate, as determined by the City Engineer, in the development of additional Lift Station capacity, and or a Water Booster Station as necessary.

20. The Developer/Owner/Applicant shall not submit an application for Final Plat until the City's Public Works Director issues a Will-Serve Letter stating the City's appurtenance has capacity to service the proposed development with domestic water, and accept the wastewater discharged from the proposed development.
21. In the event a Will-Serve Letter is not issued within the time the Applicant is required to record a Final Plat, the Applicant shall have good cause and be eligible to receive, pursuant to *KCC 5-9-203-J*, a Time Extension to file a Final Plat up to and until a Will-Serve Letter has been issued.
22. Developer/Owner/Applicant shall work with staff in order to provide final locations of streetlights as required by Kuna City Code. Streetlights for the site shall be LED lighting and must comply with Kuna City Code and established Dark Sky practices.
23. In accordance with *R90-2022*, once the Danskin Sewer Force Main is completed, 40 Equivalent Dwelling Units (EDU's) may be issued on a Phase-by-Phase basis.
24. The Developer/Owner/Applicant shall obtain written approval on letterhead or may be written/stamped on the approved plans of the construction plans from the agencies noted below. All submittals are required to include lighting, landscaping, drainage, and development plans. All site improvements are prohibited prior to approval of the following agencies:
 - A. The City Engineer shall approve all sewer connections.
 - B. The City Engineer shall approve all civil plans. No construction, grading, filling, clearing or excavation of any kind shall be initiated until the Applicant has received an approved drainage plan.
 - C. Central District Health Department recommends the plan be designed and constructed in conformance with standards contained in, "Catalog for Best Management Practices for Idaho Cities and Counties".
 - D. The Kuna Rural Fire District shall approve fire flow requirements and/or building plans. Installation of fire protection facilities as required by the Fire District are required.
 - E. The Kuna Municipal Irrigation System and Boise Project Board of Control shall approve any modifications to the existing irrigation system.
 - F. Approval from Ada County Highway District (ACHD) shall be obtained, and Impact Fees must be paid prior to issuance of any building permit(s). Please consult staff if there are questions about the process.
 - G. All public rights-of-way shall be dedicated and constructed to the standards of the City and Ada County Highway District. No public street construction may commence without the approval and permit from Ada County Highway District.
25. The Developer/Owner/Applicant, and any future assigns having an interest in the subject property, shall fully comply with all conditions of development as approved by the City Council, or seek amending them through Public Hearing processes.
26. Developer/Owner/Applicant/Contractors are hereby notified of Kuna's working hours. Construction of any kind shall only be conducted within hours specified in *KCC 10-6-3*. Noises and other public nuisances/distractions outside of this time frame are subject to lawful penalties.
27. Developer/Owner/Applicant is hereby notified of Kuna's weed control policies and requirements *KCC 8-1-3*. Weeds, grasses, vines or other growth which endanger property or are over twelve (12) inches in height shall be continuously cut down, weeded out, sprayed, burned, removed or destroyed throughout all seasons.
28. Applicant is conditioned to provide sidewalks entirely within the Rights-of-Way, compliant with Kuna City Code.
29. Applicant is conditioned to work with the City Engineer for proper easement widths for the project as a whole.
30. Applicant is conditioned to submit a Landscape plan with aa plant detail reflecting the required changes for staffs approval and be compliant with Kuna City Code.
31. Developer/Owner/Applicant and all successors shall comply with all Local, State and Federal Laws.

SECTION 5
REQUIRED NOTICES TO APPLICANT AND AFFECTED PARTIES

I.C. 6735(2)(b) Any applicant or affected person seeking judicial review of compliance with the provisions of this section must first seek reconsideration of the final decision within fourteen (14) days. Such written request must identify specific deficiencies in the decision for which reconsideration is sought. Upon reconsideration, the decision may be affirmed, reversed or modified after compliance with applicable procedural standards. A written decision shall be provided to the applicant or affected person within sixty (60) days of receipt of the request for reconsideration or the request is deemed denied. A decision shall not be deemed final for purposes of judicial review unless the process required in this subsection has been followed. The twenty-eight (28) day time frame for seeking judicial review is tolled until the date of the written decision regarding reconsideration or the expiration of the sixty (60) day reconsideration period, whichever occurs first.

BY ACTION OF THE PLANNING AND ZONING COMMISSION of the City of Kuna at its regular meeting held on the 25th day of November 2025.

Dana Hennis, Chairman

CASE NO. 25-09-S & 25-29-DR

**INDIE SUBDIVISION, SUBDIVISION AND
DESIGN REVIEW**

Planner: Marina Lundy, MLundy@kunaid.gov, (986)269-8341

**ALL APPLICATION MATERIALS: INDIE
SUBDIVISION 25-09-S & 25-29-DR**

If you require assistance accessing the application materials through the link provided above or would like to review the application materials in person at City Hall please contact the assigned planner.



**CITY OF KUNA:
P&Z COMMISSION
STAFF MEMO**

City of Kuna P&Z Commission Staff Memo

Entitlements Requested:	Final Plat	Rezone	Annexation	Special Use	Subdivision	Design Review	Time Extension
Title:	Indie Subdivision		Application Number:		25-09-S & 25-29-DR		
Date:	11.25.2025		Staff Contact:		Marina Lundy		
Owner(s)/Applicant:	Tamee Crawford – Centurion Engineers		Applicant Contact:		tlcrawford@centengr.com 208.343.3381		
Representative:	David Crawford – Centurion Engineers		Representative Contact:		dacrawford@centengr.com 208.343.3381		

Purpose
 Applicant requests subdivision and design review approval to subdivide the site into a 23-lot residential subdivision with 4 common lots. The site is approx. 5.1 acres with an existing R-6 zone. The site is located at the intersection of W Ardell Rd. and S McClure Ln., Kuna, ID (APN S1315427820).

Statement of Fact	
Parcel Number(s):	S1315427820
Future Land Use Map Designation:	Mixed Use and Medium Density Residential
Existing Land Use:	Agriculture
Current Zoning:	R-6
Proposed Zoning:	R-6
Development Area:	5.1 acres
Adjacent Zoning Districts:	North: A East: RUT South: R-3 West: R-6
Adjacent Street(s) Existing & Proposed:	North: W Ardell Rd. East: N McClure Ln. (Private) South: W Gainsboro Dr. & N Mauve Ave West: W Henna St. & W Beige St.
Internal Street(s) Existing & Proposed:	Reliant St. (Proposed)
Adjacent Bike/Pedestrian Facilities:	None
Adjacent Parks:	None
Land Dedication Requirements:	N/A

Comprehensive Plan and Future Land Use Map Analysis
 The Comprehensive Plan identifies the subject property as Mixed Use and Medium Density Residential, the existing zone is a City of Kuna Zone; R-6. Since commercial development is most appropriate and beneficial at the hard corner of W. Ardell Road and N. McClure Lane, the northern portion of this parcel designated as

Mixed-Use on the Future Land Use Map, is not required to develop as Commercial or to request a Comprehensive Plan Map Amendment. The proposed zoning and land use for the subject site agrees with the Future Land Use Map (FLUM) of the City of Kuna.

Staff Analysis

The applicant requests preliminary plat and design review approval to subdivide the subject site of 5.1 acres into 23 residential lots and 4 common lots.

Upon staff review, the proposed application meets the requirements and intent of Kuna City Code and Comprehensive Plan. Staff recommends the Commission approve the application with the conditions outlined in staff's report.

Recommended Conditions of Approval

1. Developer/Owner/Applicant shall work with Ada County Highway District and the City of Kuna to complete all required traffic improvements to provide public access to the site as this project will have internal private streets, as detailed in the Ada County Highway District staff report.
2. Developer/Owner/Applicant shall measure all front building setbacks from back of sidewalk on all internal local roads.
3. Applicant shall ensure the proper easement widths on all lots in accordance with KCC 5-9-308.
4. It is the responsibility of the Developer to ensure any anticipated buildings fit any given buildable lot.
5. Fencing within and around the site shall comply with KCC 5-8-1005 (unless specifically approved otherwise and permitted).
6. All required landscaping shall be permanently maintained in a healthy condition. The property Owner shall remove and replace unhealthy or dead plant material within 3 days or as the planting season permits as required to meet the standards of these requirements. Maintenance and planting in public Rights-of-Way shall be with approval from ACHD.
7. Landscaping shall not be placed within ten (10) feet of any meter pits, pressurized irrigation valves and/or ACHD underground facilities and must honor all vision triangles.
8. All signage within/for the project shall comply with Kuna City Code and shall be approved through the applicable sign approval process listed in KCC 5-8-1704 through 1706.
9. If any revisions are made, the Applicant shall provide the Planning and Zoning Staff with a revised copy of the Preliminary Plat. Any revisions of the Plat are subject to Administrative Determination to rule if the revision is substantial.

10. Developer/Owner/Applicant is hereby notified this project is subject to Design Review inspections and fees. Required inspections (post construction), are to verify building and landscaping compliance prior to requesting signature on the final plat.
11. Installation of service facilities shall comply with the requirements of the public utility or irrigation district providing the services. All utilities shall be installed underground, see KCC 5-9-402-B-22.
12. Connection to City Services (Sewer and Water) is required. The Applicant shall conform to all corresponding City of Kuna Master Plans.
13. Developer/Owner/Applicant shall work with staff in order to provide final locations of streetlights as required by Kuna City Code. Streetlights for the site shall be LED lighting and must comply with Kuna City Code and established Dark Sky practices.
14. The Developer/Owner/Applicant, and any future assigns having an interest in the subject property, shall fully comply with all conditions of development as approved by the City Council, or seek amending them through Public Hearing processes.
15. Developer/Owner/Applicant/Contractors are hereby notified of Kuna's working hours. Construction of any kind shall only be conducted within hours specified in KCC 10-6-3. Noises and other public nuisances/distractions outside of this time frame are subject to lawful penalties
16. Developer/Owner/Applicant is hereby notified of Kuna's weed control policies and requirements KCC 8-1-3. Weeds, grasses, vines or other growth which endanger property or are over 12 inches in height shall be continuously cut down, weeded out, sprayed, burned, removed or destroyed throughout all seasons.
17. Upon City Council's approval, no revisions shall be made to the Final Plat. If revisions are desired, the Applicant shall bring a copy of the changes to Planning and Zoning staff to determine if a new approval is required via the Public Hearing process.
18. Correct and verified as-built (record drawings) drawings shall be required before occupancy or Final Plat approval by City Engineer is granted.
19. Applicant is conditioned to work with the City Engineer for proper easement widths for the project as a whole.
20. Developer/Owner/Applicant and all successors shall comply with all Local, State and Federal Laws.

City of Kuna

Planning and Zoning Department
751 W. 4th Street
P.O. Box 13
Kuna, ID 83634

July 31, 2025

Dear Mayor, City Council, Planning and Zoning Commission, and Staff,

On behalf of our client, Ryan Recla, we are pleased to present Indie Subdivision. Our clients' goal is to thoughtfully create an infill subdivision with a mix of lots sizes ranging from 4,460 square feet to 12,446 square feet in area.

The Site. The 5.25-acre property is annexed into the City of Kuna and zoned R-6. A unique feature of the site is its limited frontage on West Ardell Road and two stub streets. The project site is a single Assessor's Parcel Number(S1315424820).

The Project. We are requesting preliminary plat approval. The proposed subdivision complies with the provisions of the R-6 zone.

While the project does have frontage on Ardell Road, that frontage is proposed as emergency access only. We were unable to locate a street entrance that would meet ACHD spacing requirements for access. West Henna Street stubs into the property's west boundary from Crimson Point North Subdivision No. 3. This stub is being continued to the east boundary where it will stub into the Amended Plat of McClure Subdivision. North Mauve Avenue stubs into the property's south boundary from Crimson Point Subdivision Phase 5. This stub is being continue northeasterly toward the north boundary.

There are three landscape lots: one along Well Ardell and two along North Mauve Avenue. Lot 20 Block 2 is a nearly quarter-acre open space lot to be enjoyed by the residents. The lot is centrally located along the east boundary of the property. The landscape and open space lots will be held in common ownership by the homeowners' association.

We look forward to answering any questions you may have about the proposal.

Sincerely,



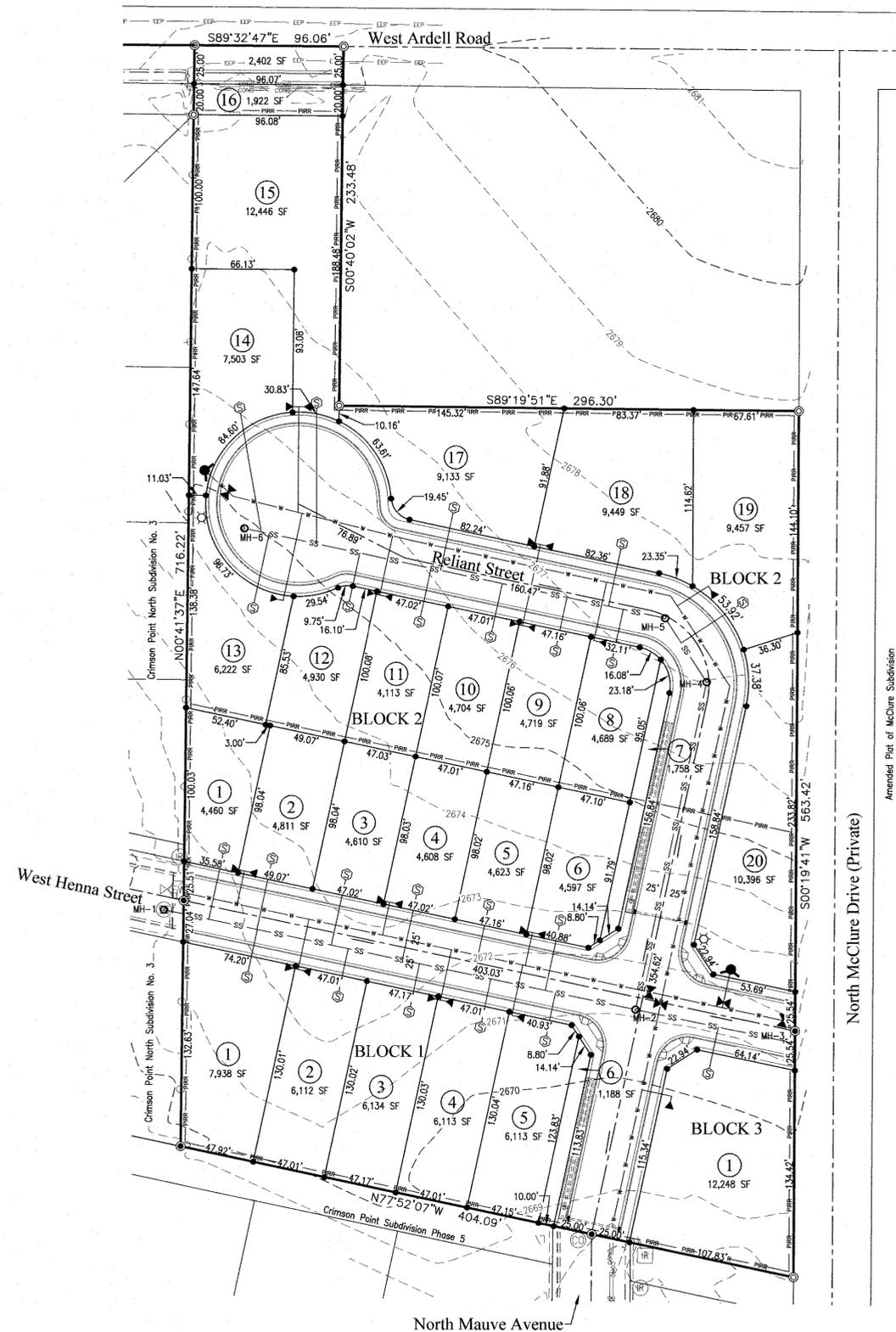
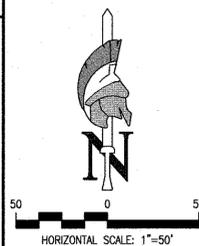
David Crawford
Project Manager
Centurion Engineers, Inc.
dacrawford@centengr.com

Vicinity Map



Notes

1. THERE ARE NO POTENTIALLY HAZARDOUS AREAS.
2. THERE ARE NO STREAMS, PONDS, LAKES, OR WETLANDS ON THIS PROPERTY.
3. THERE ARE NO NATURAL DRAINAGE SWALES ON THIS PROPERTY.
4. THERE ARE NO PERMANENT AND/OR SEASONAL HIGH GROUNDWATER AREAS ON THIS PROPERTY.
5. THIS PROPERTY IS NOT IN A FLOOD PLAIN AREA.
6. THERE ARE NO IDENTIFIED SHALLOW BEDROCK AREAS, UNSTABLE ROCK FORMATIONS, OR LANDSLIDE AREAS ON THIS PROPERTY.
7. THIS PROPERTY IS NOT IN AN AQUIFER RECHARGE AREA.
8. NO UNSTABLE SOILS SUSCEPTIBLE TO EROSION EXISTING ON THIS PROPERTY. ALL AREAS ARE SUITABLE FOR DEVELOPMENT.
9. SANITARY SEWER SERVICE IS TO BE PROVIDED BY THE CITY OF KUNA.
10. POTABLE WATER IS TO BE PROVIDED BY THE CITY OF KUNA.
11. PROPERTY LIES WITHIN THE BOISE-KUNA/ NEW YORK IRRIGATION DISTRICT.
12. PROPERTY LIES WITHIN THE KUNA CITY FIRE DISTRICT.
13. EXISTING IRRIGATION FACILITIES TO BE CONTINUED TO THEIR HISTORIC DISCHARGE POINTS.
14. THIS PROPERTY IS ZONED R-6.
15. EXISTING USE: AGRICULTURAL.
16. CONTOUR INTERVAL: 2 FOOT DATUM: NAVD 88
17. COMMON AREAS SHOWN HEREON SHALL BE MAINTAINED BY THE HOMEOWNERS' ASSOCIATION.
18. THIS DEVELOPMENT ANTICIPATES USING SUBSURFACE STORM WATER DISPOSAL OF STORM WATER GENERATED FROM THE LOCAL ROAD SYSTEM AND LANDS TRIBUTARY TO THE SYSTEM.
19. PUBLIC UTILITIES ARE TO BE PROVIDED FROM THE PUBLIC UTILITY PROVIDERS FROM JOINT TRENCH ADJACENT TO THE PUBLIC ROADS. SPECIFIC DESIGN CRITERIA WILL BE MET DURING THE CONSTRUCTION APPROVAL PHASE OF THIS DEVELOPMENT.
20. THIS PARCEL LIES WITHIN THE "ZONE X" FLOODWAY PER FEMA MAP #160010250J, PANEL NOT PRINTED.
21. THE FOLLOWING TAX PARCEL NUMBER IS INCLUDED WITHIN THIS SUBDIVISION: S1315424820.
22. POTABLE WATER, SANITARY SEWER AND PRESSURE IRRIGATION ARE AVAILABLE TO THIS SITE WITH THE EXTENSION OF SERVICES FROM MAINLINES AS REQUIRED.
23. ALL STORM WATER GENERATED BY THIS DEVELOPMENT WILL BE RETAINED ON SITE AND/OR BY ACCEPTED STORM WATER DISPOSAL METHODS.
24. MINIMUM BUILDING SETBACKS AND DIMENSIONAL STANDARDS WITHIN THIS SUBDIVISION SHALL COMPLY WITH THE APPLICABLE ZONING ORDINANCE AT THE TIME OF ISSUANCE OF THE BUILDING PERMITS.
25. LOT 6, BLOCK 1, AND LOTS 7, 16, AND 20, BLOCK 2 ARE COMMON LOTS TO BE OWNED AND MAINTAINED BY THE HOA.



Amended Part of McClure Subdivision

North McClure Drive (Private)

Manhole Invert Table

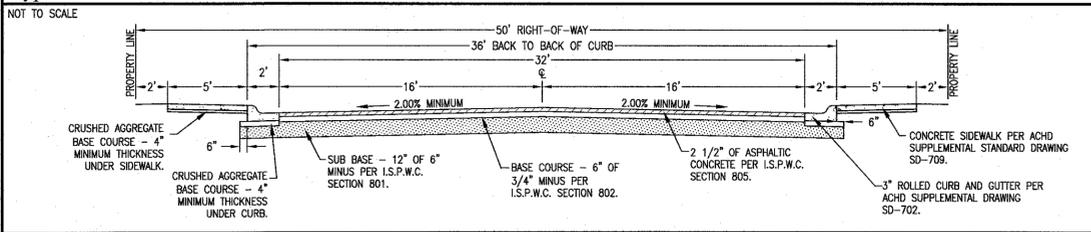
MH #	INVERTS	RIM & DEPTH
MH-1	8" INV IN=2664.13 (E)	RIM=2670.88 DEPTH=6.75'
MH-2	8" INV IN=2665.47 (E) 8" INV IN=2665.47 (N) 8" INV OUT=2665.37 (W)	RIM=2671.14 DEPTH=5.77'
MH-3	8" INV OUT=2666.50 (W)	RIM=2671.33 DEPTH=5.45'
MH-4	8" INV IN=2666.44 (NW) 8" INV OUT=2666.34 (S)	RIM=2676.98 DEPTH=10.64'
MH-5	8" INV IN=2666.73 (W) 8" INV OUT=2666.63 (SE)	RIM=2677.36 DEPTH=10.73'
MH-6	8" INV OUT=2668.40 (E)	RIM=2674.83 DEPTH=6.43'

THE AFFIXED STAMP INDICATES THAT THE SUBDIVISION BOUNDARY SHOWN HEREON IS THE ACTUAL FIELD SURVEY.

Legend

- SUBDIVISION BOUNDARY
- LOT LINE
- STREET CENTERLINE
- SECTION LINE
- EXISTING FENCE
- EEP EEP EEP EEP — EXISTING EDGE OF PAVEMENT
- CONC CONC CONC CONC — EDGE OF CONCRETE
- EXISTING CURB AND GUTTER
- CONSTRUCT 3" ROLLED CURB & GUTTER
- EX-SS — EX-SS — EX-SS — EX-SS — EXISTING SANITARY SEWER MAIN LINE
- SS — SS — SS — SS — CONSTRUCT SANITARY SEWER MAIN LINE, SIZE NOTED
- EX-W — EX-W — EX-W — EX-W — EXISTING WATER MAIN LINE
- W — W — W — W — CONSTRUCT WATER MAIN LINE, SEE SHEET 3.2 FOR SIZES
- PIRR — PIRR — PIRR — PIRR — CONSTRUCT WATER SERVICE PER CITY OF KUNA SD-K401.75
- PIRR — PIRR — PIRR — PIRR — CONSTRUCT PRESSURE IRRIGATION MAIN LINE
- INSTALL WATER VALVE, SAME SIZE AS MAIN LINE
- INSTALL FIRE HYDRANT
- EXISTING WATER VALVE
- EXISTING WATER METER
- INSTALL LED STREET LIGHT
- LOT NUMBER
- INSTALL SANITARY SEWER MANHOLE
- EXISTING SANITARY SEWER MANHOLE
- EXISTING SANITARY SEWER CLEAN OUT
- EXISTING IRRIGATION BOX
- EXISTING PI VALVE
- FOUND 5/8" PIN WITH CAP LABELED AS NOTED
- SET 1/2"x24" IRON PIN WITH PLASTIC CAP LABELED "CENTURION PLS 4116"
- SET 5/8"x24" IRON PIN WITH PLASTIC CAP LABELED "CENTURION PLS 4116"
- STORM DRAIN LOCATION
- GRADE DIRECTION

Typical 50-Foot Local Street Section



Setback/Zoning Table

EXISTING ZONE	R-6
MINIMUM LOT AREA	4,000 SF
FRONT YARD SETBACK	20 FEET
REAR YARD SETBACK	15 FEET
STREET SIDE YARD SETBACK	15 FEET
INTERIOR SIDE YARD SETBACK	5 FEET
MAXIMUM BUILDING HEIGHT	35 FEET
MINIMUM STREET FRONTAGE	45 FEET
MAXIMUM DENSITY	6.0/ACRE

Land Use Calculations

DESCRIPTION	AREA (ACRES)	PERCENTAGE
PROJECT	5.25	100%
RESIDENTIAL LOTS	3.42	65.1%
OPEN SPACE AREA LOTS	0.44	8.4%
RIGHT-OF-WAY	1.39	26.5%
RESIDENTIAL LOTS	23	
OPEN SPACE AREA LOTS	5	
RESIDENTIAL DENSITY	4.4/Acre	

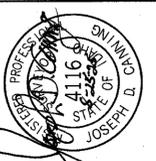
Owner/Applicant

SJR HOLDINGS, LLC
LMS@RECLAIMNETSCAPE.NET
(208) 327-7711

Underground Utility Note

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. DEVIATIONS MAY EXIST BETWEEN THE LOCATIONS SHOWN HEREON AND THEIR ACTUAL LOCATION(S).

CENTURION ENGINEERS, INC.
Consulting Engineers, Land Surveyors, Planners
2323 S. Vista Ave., Suite 206 | Boise, ID 83705
208.343.3381 | www.centengr.com



Indie Subdivision
A PORTION OF LAND SITUATE IN THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 15, TOWNSHIP 2 NORTH, RANGE 1 WEST, BOISE MERIDIAN, KUNA, ADA COUNTY, IDAHO.

Preliminary Plat

SCALE: 1" = 50'
DATE: JUNE 10, 2025
DRAWN BY: K.R. CANNING
CHECKED BY: J.D. CANNING
PROJECT NO.: PRZ1
DRAWING FILE NAME: PRZ1_Prelim_Plat.dwg

SHEET NO.: **1.1**

Plant Palette Table

SYM COMMON NAME

EVERGREEN TREES

- BH BLACKHILLS SPRUCE
- CB COLUMNAR BLUE SPRUCE
- VP VANDERWOLF PINE

SHADE TREES (CLASS 2)

- SH SUNBURST HONEYLOCUST
- AA AUTUMN PURPLE ASH
- SM SUNSET MAPLE

ORNAMENTAL TREES (CLASS 1)

- AM AMUR MAPLE
- AP ARISTOCRATIC PEAR
- PC PRAIRIEFIRE CRABAPPLE

SHRUBS/ORNAMENTAL GRASSES/PERENIALS

- BE BLACK EYE SUSAN
- CP CRIMSON PUGMY BARBERRY
- CM CREEPING MAHONIA
- RF RED FLOWER CARPET ROSE
- DF DWARF FOUNTAIN GRASS
- DG DARTS GOLD NINEBARK
- ES ENDLESS SUMMER HYDRANGEA
- LD LITTLE DEIL NINEBARK
- MG MAIDEN GRASS
- SF SHOWOFF FORSYTHIA
- SN SUMBERWINE NINEBARK

3 TOTAL SHRUBS
3 TOTAL GRASSES
FROM TABLE

Notes

- ALL PLANTS SHALL MEET OR EXCEED MINIMUM KUNA CITY ORDINANCE REQUIREMENTS.
- ALL PLANTING AREAS TO BE WATERED WITH AN AUTOMATIC UNDERGROUND IRRIGATION SYSTEM.
- ALL COMMON SPACE LANDSCAPING SHALL BE OWNED AND MAINTAINED BY A HOMEOWNER'S ASSOCIATION.
- NO TREES SHALL IMPEDE THE 40-FOOT VISION TRIANGLES AT ROAD INTERSECTIONS. NO TREES OR SHRUBS OVER 3- FEET HIGH AT MATURITY SHALL BE PLANTED WITHIN STREET VISION TRIANGLES.
- NO TREES SHALL BE PLANTED WITHIN 10- FEET OF ANY ADA COUNTY HIGHWAY DISTRICT STORM DRAINAGE FACILITIES. NO TREES SHALL BE PLANTED WITHIN 50- FEET OF STOP SIGNS.
- INSTALLATION OF REQUIRED PLANTING ON THE RESIDENTIAL LOTS SHALL BE COMPLETED AS REQUIRED AT THE TIME OF HOME CONSTRUCTION.
- ALL COMMON LOTS SHALL BE PLANTED WITH HYDROSEED OR SOD AFTER TREE PLANTING.

Buffer Calculations

LANDSCAPE BUFFER ARDELL ROAD
TOTAL LENGTH 96 L.F.

REQUIRED	INSTALLED	# OF SPECIES
2 SHADE	2	6
3 EVERGREENS	3	2
12 SHRUBS/ORNAMENTAL GRASSES	12	13

*SHADE TREES CAN BE REPLACED AT A 2:1 RATION WITH EVERGREEN/ORNAMENTAL TREES

LANDSCAPE BUFFER LOCAL STREETS
TOTAL LENGTH 756 L.F.

REQUIRED	INSTALLED	# OF SPECIES
N/A SHRUBS/ORNAMENTAL GRASSES	14	3
N/A ORNAMENTAL*	14	11

*SHADE TREES CAN BE REPLACED AT A 2:1 RATION WITH EVERGREEN/ORNAMENTAL TREES

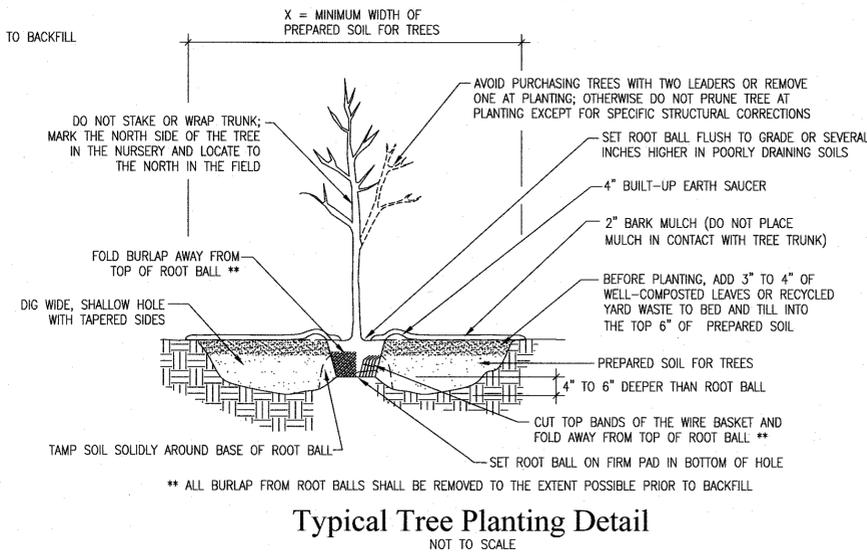
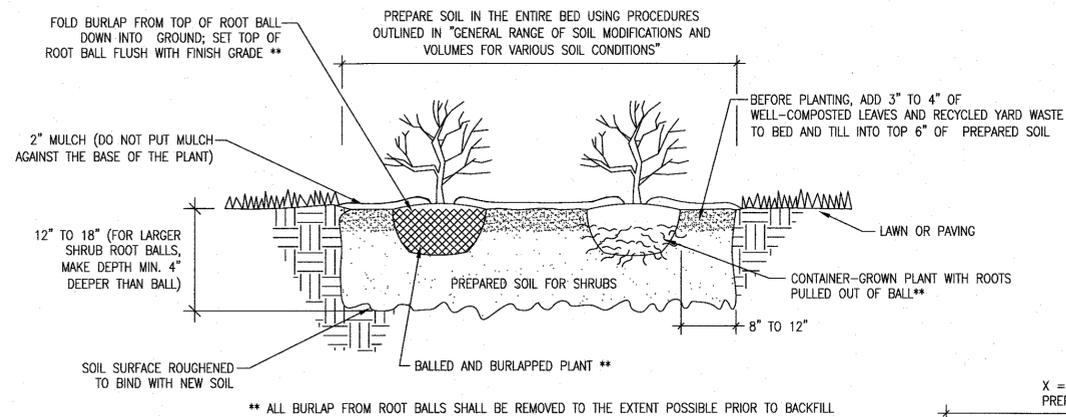
Open Space Calculations

REQUIRED	SHADE	EVERGREEN	ORNAMENTAL
11 LOT 20, BLOCK 2	5	11	4

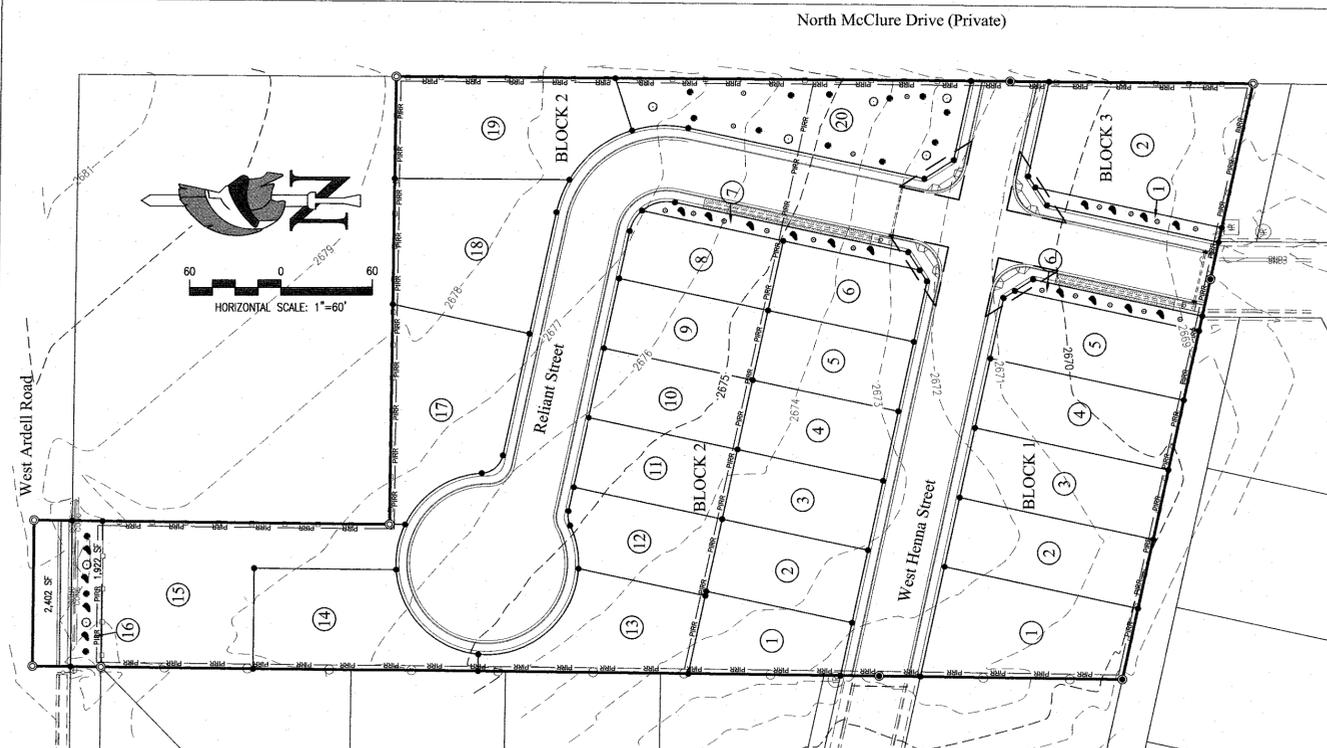
*SHADE TREES CAN BE REPLACED AT A 2:1 RATION WITH EVERGREEN/ORNAMENTAL TREES

Fencing

6-FOOT VINYL PRIVACY FENCING SHALL BE INSTALLED ALONG THE UNFENCED PORTIONS OF THE BOUNDARY OF THE SUBDIVISION, ALONG ALL REAR LOT LINES
PHASE 1 TOTAL LINEAL FOOTAGE = 1,427 L.F.



Vicinity Map



CENTURION ENGINEERS
Consulting Engineers, Land Surveyors, Planners
2323 S. Vista Ave. Suite 206 | Boise, ID 83705
208.343.3381 | www.centengr.com

Landscape Plan
Indie Subdivision
A PORTION OF LAND SITUATE IN THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 15, TOWNSHIP 2 NORTH, RANGE 1 WEST, BOISE MERIDIAN, KUNA, ADA COUNTY, IDAHO.

REV.	DESC.	DATE	BY

SCALE: 1" = 50'
DATE: JUNE 10, 2025
DRAWN BY: K.C. CANNING
CHECKED BY: J.D. CANNING
PROJECT NO: RR21
DRAWING FILE NAME: RR21_P16_Fencing

SHEET NO:
L1.0



Miranda Gold, President
 Alexis Pickering, Vice-President
 Kent Goldthorpe, Commissioner
 Dave McKinney, Commissioner
 Patricia Nilsson, Commissioner

Date: October 29, 2025

To: Tamee Crawford, Centurion Engineers

Staff Contact: Sam Standal, Assistant Traffic Engineer

Project Description: Indie Subdivision

Trip Generation: This development is estimated to generate 209 vehicle trips per day, 21 vehicle trip per hour in the PM peak hour, based on the Institute of Transportation Engineers Trip Generation Manual, 12th edition.

Proposed Development Meets	
All ACHD Policies	
Requires Revisions to meet ACHD Policies	X

Traffic Impact Study	
Yes	
No	X
If yes, is mitigation required	

Area Roadway Level of Service	
Do area roadways meet ACHD's LOS Planning Thresholds?	
Yes	X
No	
Area roads will meet ACHD's LOS Planning Thresholds in the future with planned improvements?	
Yes	X
No	

ACHD Planned Improvements	
FYP	X
CIP	X

Livable Street Performance Measures	
Pedestrian	LTS 1
Cyclist	LTS 1

Is Transit Available?	
Yes	
No	X

Comments:

connecting you to more



Project/File: **Indie Subdivision / KPP25-0012 / 25-09-S 25-29-DR**
 This is a preliminary plat and design review application for a 28-lot subdivision, consisting of 23-residential and 5 common lots on 5.25 acres.

Lead Agency: City of Kuna

Site address: South of Ardell Road, East of Henna Street

Staff Approval: October 29, 2025

Applicant: Tamee Crawford
 Centurion Engineers, Inc
 2323 S Vista Avenue, Suite 206
 Boise, ID 83705

Representative: David Crawford
 Centurion Engineers, Inc
 2323 S Vista Avenue, Suite 206
 Boise, ID 83705

Staff Contact: Sam Standal
 Phone: 208-387-6384
 E-mail: sstandal@achdidaho.org

Report Summary:

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ACHD Planned Improvements

1. Capital Improvements Plan (CIP)/ Five Year Plan (FYP):

- The intersection of Hubbard Road and Ten Mile Road is scheduled in the FYP to be reconstructed as a single-lane roundabout, with design occurring in 2028-2029 and construction at a future date.
- Ten Mile Road is listed in the CIP to be widened to 3-lanes from Amity Road to Deer Flat Road between 2031 and 2040.
- The intersection of Deer Flat Road and Ten Mile Road is listed in the CIP to be reconstructed as a single lane roundabout between 2026 and 2030.

Level of Service Planning Thresholds

1. Condition of Area Roadways

Traffic Count is based on Vehicles per hour (VPH)

Roadway	Frontage	Functional Classification	PM Peak Hour Traffic Count	PM Peak Hour Level of Service
Ardell Road	96-feet	Collector	117	Better than "D"
Henna Street	52-feet	Local	N/A	N/A
Mauve Avenue	52-feet	Local	N/A	N/A

* Acceptable level of service for a two-lane collector is "D" (425 VPH).

2. Average Daily Traffic Count (VDT)

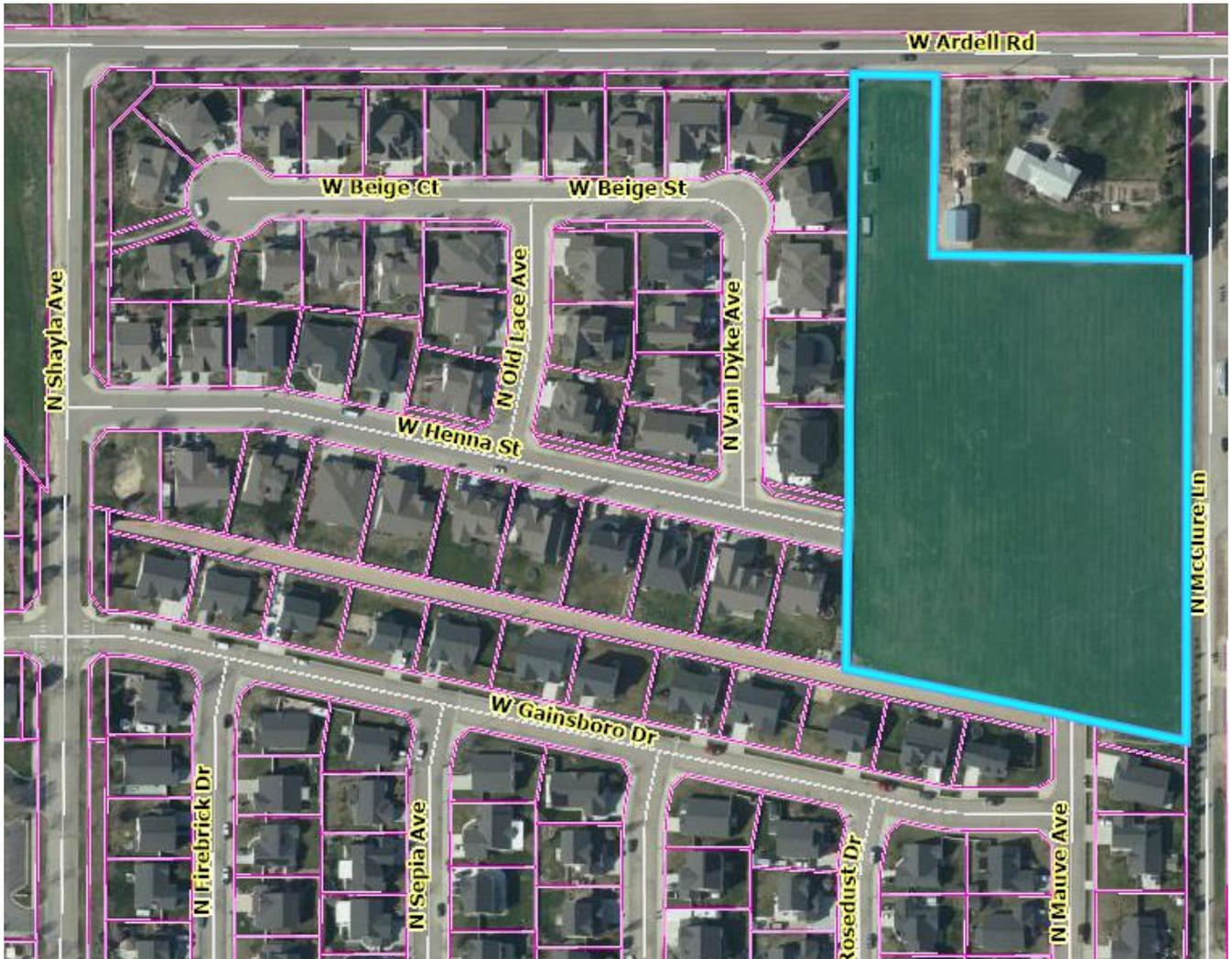
Average daily traffic counts are based on ACHD's most current traffic counts.

- The average daily traffic count for Ardell Road west of Ten Mile Road was 2,027 on June 13, 2024.
- There are no current traffic counts for Henna Street or Mauve Avenue abutting the site.

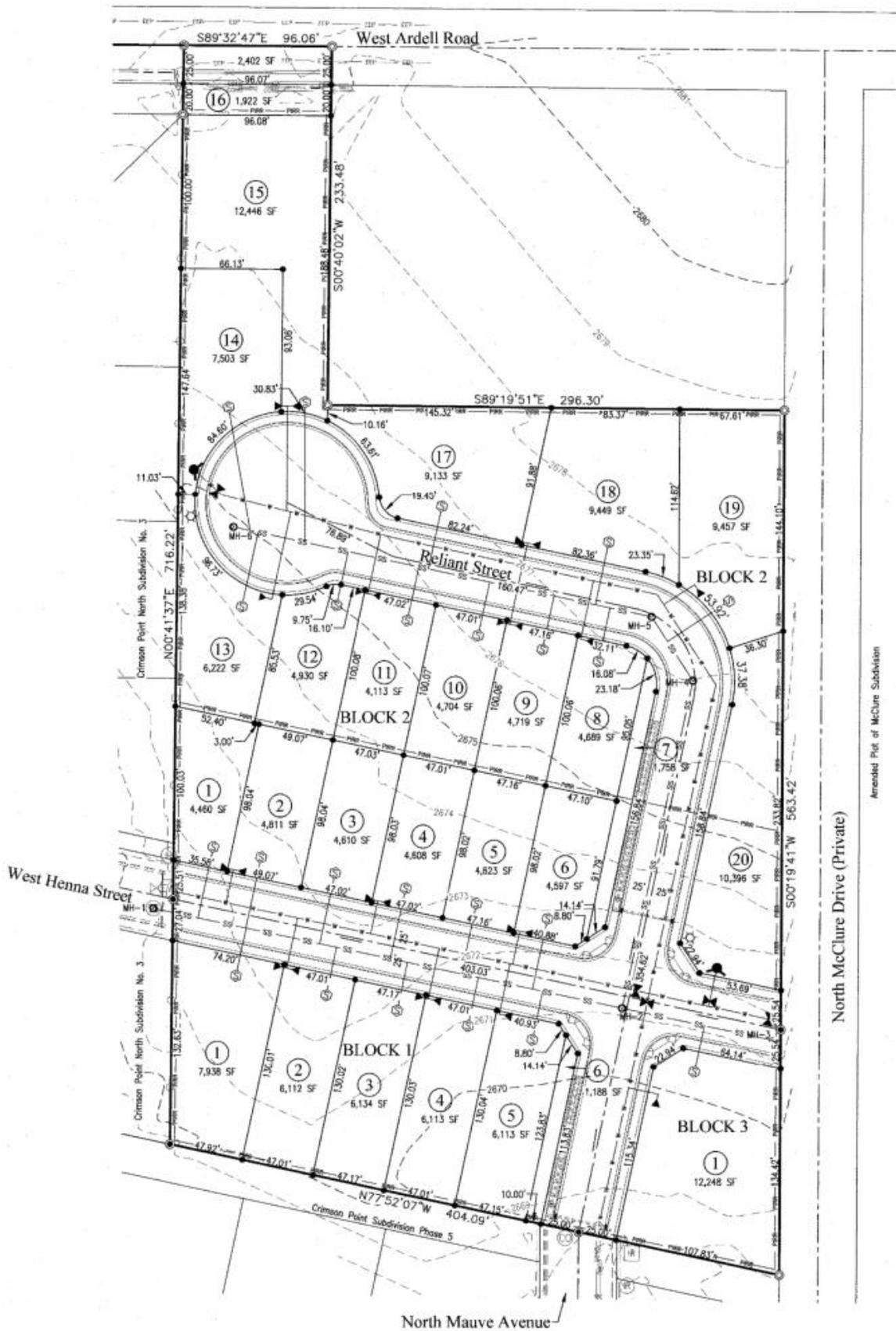
A. Site Specific Conditions of Approval

1. Construct Ardell Road as half of a 36-foot-wide collector street section with vertical curb, gutter, and 7-foot-wide attached or 5-foot-wide detached concrete sidewalk. If detached sidewalk is desired, then provide minimum 8-foot-wide landscape buffer.
2. Dedicate right-of-way on Ardell Road extending to 2-feet behind the back of sidewalk. For detached sidewalk, the applicant may dedicate right-of-way extending 2-feet behind the back of curb and provide a right-of-way easement from the edge of right-of-way to 2-feet behind the back of sidewalk.
3. Extend Henna Street and Mauve Avenue into the site and construct the internal streets as 36-foot-wide local street sections with rolled curb, gutter, and 5-foot-wide attached concrete sidewalk.
4. Terminate Reliant Street in a minimum 50-foot radius cul-de-sac turnaround with rolled curb, gutter and 5-foot-wide attached concrete sidewalk.
5. Dedicate 50-feet of right-of-way for all internal streets and right-of-way extending to 2-feet behind the back of sidewalk around the cul-de-sac turnaround.
6. Stub Henna Street to the site's east property line, located 636-feet south of Ardell Road. Install a sign at the terminus of Henna Road stating "THIS ROAD WILL BE EXTENDED IN THE FUTURE".
7. Other than the access specifically approved with this application, direct lot access is prohibited to Ardell Road and should be noted on the final plat.
8. Submit civil plans to ACHD Development Services for review and approval. The impact fee assessment will not be released until the civil plans are approved by ACHD.
9. There will be an impact fee that is assessed and due prior to issuance of any building permits. The assessed impact fee will be based on the impact fee ordinance that is in effect at that time. The impact fee assessment will not be released until the civil plans are approved by ACHD.
10. Comply with all Standard Conditions of Approval.

B. Vicinity Map



C. Site Plan



D. Findings for Consideration

1. Ardell Road

- a. Existing Conditions:** Ardell Road is improved with 2-travel lanes, 28-feet of pavement, and no curb, gutter or sidewalk abutting the site. There is 48 to 51-feet of right-of-way for Ardell Road (25 to 28-feet from centerline).
- b. Applicant Proposal:** The applicant is proposing to construct Ardell Road as half of a 36-foot-wide collector street section with rolled curb, gutter, and 5-foot-wide attached concrete sidewalk. In addition, the applicant is proposing to dedicate right-of-way totaling 25-feet from centerline.
- c. Staff Comments/Recommendations:** The applicant's proposal to improve Ardell Road generally meets District Policy with the exception of the curb type and sidewalk width. The applicant should be required to improve Ardell Road with vertical curb and either 7-foot-wide attached or 5-foot-wide detached sidewalk to tie into existing improvements west of the site. If detached sidewalk is desired, then a minimum 8-foot-wide planter strip should be provided.

The applicant should be required to dedicate right-of-way on Ardell Road extending to 2-feet behind the back of sidewalk. For detached sidewalk, the applicant may dedicate right-of-way extending 2-feet behind the back of curb and provide a right-of-way easement from the edge of right-of-way to 2-feet behind the back of sidewalk.

2. Local Streets

- a. Existing Conditions:** There are two existing stub street, Henna Street and Mauver Avenue which stub to the site's west and south property lines. The stub streets were constructed as part of Crimson Point North and Crimson Point Subdivision.
- b. Applicant's Proposal:** The applicant is proposing to extend Henna Avenue and Mauve Avenue into the site and to construct the internal local streets as 36-foot-wide local street sections with rolled curb, gutter, and 5-foot-wide attached concrete sidewalk. Reliant Street is proposed to terminate in a cul-de-sac turnaround with curb, gutter, and 5-foot-wide attached concrete sidewalk.

The applicant is proposing to dedicate 50-feet of right-of-way for all internal streets and to dedicate right-of-way extending to 2-feet behind the back of sidewalk around the cul-de-sac turnaround.

- c. Staff Comments/Recommendations:** The applicant's proposal meets District Policy and should be approved as proposed. The cul-de-sac turnaround at the terminus of Reliant Street should be constructed with a minimum radius of 50-feet.

3. Stub Streets

- a. Existing Conditions:** There are two existing stub street, Henna Street and Mauver Avenue which stub to the site's west and south property lines. The stub streets were constructed as part of Crimson Point North and Crimson Point Subdivision.
- b. Applicant Proposal:** The applicant is proposing to extend Henna Street through the site stubbing to the site's east property line, located 636-feet south of Ardell Road.
- c. Staff Comments/Recommendations:** The applicant's proposal meets District Policy and should be approved. The applicant should be required to install a sign at the terminus of Henna Street stating "THIS ROAD WILL BE EXTENDED IN THE FUTURE."

4. Private Road

There is an existing private road, McClure Lane abutting the site's east property line. The private road was constructed as part of McClure Lane Subdivision which final platted in 1991 and is owned by the McClure Lane Neighborhood Association. The applicant has proposed to construct one stub street, Henna Street, to stub to the private road, however no access is proposed or should be taken to the private street as part of this development.

5. Other Access

Ardell Road is classified as a collector roadway. Other than the access specifically approved with this application, direct lot access is prohibited to this roadway and should be noted on the final plat.

E. Policy

1. Federal Accessibility Design Guidelines and Standards

District policy 7203.1.1 states that developers shall follow the current version of the U.S. Access Board's Accessibility Guidelines for Pedestrian Facilities in the Public Right-of-Way (PROWAG), 36 CFR Part 1190, September 7, 2023; (Also see, <https://www.access-board.gov/prowag> and <https://www.access-board.gov/files/prowag/planning-and-design-for-alternatives.pdf> for additional information).

2. Minor Improvements

Minor Improvements Policy: District Policy 7203.3 states that minor improvements to existing Highways adjacent to a proposed development may be required. These needed transportation facilities are to correct deficiencies or replace deteriorated facilities. Included are sidewalk and/or bike lane construction or replacement (with physical buffers if missing and needed); construction of transitional sidewalk segments; crosswalk construction or replacement; curb and gutter construction or replacement; repair, replacement or expansion of curb extensions; replacement of unused driveways with curb, gutter, sidewalk, repair or addition of traffic calming or speed mitigation features; installation or reconstruction of pedestrian ramps; pavement repairs; signs, motor vehicle, pedestrian and bicycle traffic control devices; and other similar items. The current version of PROWAG will determine the applicable accessibility requirements for alterations and elements added to existing streets. ACHD staff is responsible for identifying the minor improvements that would be proportionate to the size and complexity of the development.

3. Livable Street Performance Measures

District policy 7203.8 states that ACHD has adopted performance measures (level of stress) for evaluating the experience of bicyclists and pedestrians. ACHD seeks to create a transportation network that serves all ages and abilities. Bike and pedestrian facilities built through development should achieve a bike and pedestrian level of traffic stress 1 or 2, with no new bike lane below a minimum of 5-feet.

4. Ardell Road

Collector Street Policy: District policy 7206.2.1 states that the developer is responsible for improving all collector frontages adjacent to the site or internal to the development as required below, regardless of whether access is taken to all of the adjacent streets. Improvements shall include transitional segments in accordance with ADA and the current version of PROWAG.

Master Street Map and Typologies Policy: District policy 7206.5 states that if the collector street is designated with a typology on the Master Street Map, that typology shall be considered for the required street improvements. If there is no typology listed in the Master Street Map, then standard street sections shall serve as the default. ACHD reserves the right to require bicycle and pedestrian facilities above those identified in the Livable Streets Design Guide and Master Street Map to ensure the safest facility possible based on current best practice.

Street Section and Right-of-Way Policy: District policy 7206.5.2 states that the standard right-of-way width for collector streets shall typically be 50 to 70-feet, depending on the location and width of the sidewalk or multi-use path and the location and use of the roadway. The right-of-way width may be reduced, with District approval, if the sidewalk or multi-use path is located within an easement; in which case the District will require a minimum right-of-way width that extends 2-feet behind the back-of-curb on each side.

The standard street section shall be 47-feet (back-of-curb to back-of-curb). This width typically accommodates a single travel lane in each direction, a continuous center left-turn lane, and bike lanes. If a multi-use path is determined to be the required treatment, the street section shall be reduced to 37-feet. If it is determined a continuous center left-turn lane is not appropriate and a path is installed, the street section may be reduced to 26-feet.

Residential Collector Policy: District policy 7206.5.2 states that the standard street section for a collector in a residential area shall be 36-feet (back-of-curb to back-of-curb). The District will consider a 33-foot or 29-foot street section with written fire department approval and taking into consideration the needs of the adjacent land use, the projected volumes, the need for bicycle lanes, and on-street parking.

At the discretion of Traffic Engineering staff, adjustments may be made to the street section, including removal of the continuous center turn lanes or modification to lane widths, where no driveways or intersections are present or to ensure adequate space for pathways and buffers.

Pedestrian Facilities: District policy 7206.5.6 requires a concrete sidewalk at least 5-feet wide to be constructed on both sides of all collector streets. A parkway strip at least 8-feet wide between the back-of-curb and street edge of the sidewalk is required to provide increased safety and protection of pedestrians. Consult the District’s planter width policy if trees are to be placed within the parkway strip. Sidewalk constructed next to the back-of-curb shall be a minimum of 7-feet wide. Alternatively, on roadways identified as low-stress bikeways in ACHD’s Bike Master Plan, a minimum 10-foot wide multi-use path may be required in lieu of sidewalks. ACHD Development Review staff will be responsible for determining the required facility. The path shall be built 8-feet behind the back-of-curb as measured to the closest edge of the path. Street trees are encouraged between the pedestrian facility and the roadway when irrigation and maintenance will occur by the adjacent property owner or HOA through an approved license agreement. Vertical hardscape alternatives to street trees may be considered in the buffer space when street trees are not practicable.

Pedestrian facilities should be parallel to the adjacent roadway. Pedestrian facilities will only be allowed to deviate from a straight line when authorized by Development Review staff to meet site specific conditions (i.e., street trees, utilities, etc.).

Appropriate easements shall be provided if public sidewalks or multi-use paths are placed out of the right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk or multi-use path. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

Curb ramps or blended transitions shall be provided to connect the pedestrian access route at each pedestrian street crossing in accordance with the current version of PROWAG. Provide detectable warning surface in accordance with the current version of PROWAG.

Ardell Road ACHD Master Street Map: ACHD Policy Section 3111.1 requires the Master Street Map (MSM) guide the right-of-way acquisition, collector street requirements, and specific roadway features required through development. This segment of Ardell Road is designated in the MSM as a Residential Collector with 2-lanes and on-street bike lanes, a 36-foot street section within 50-feet of right-of-way.

5. Henna Street, Mauve Avenue, and Reliant Street

Local Roadway Policy: District Policy 7207.2.1 states that the developer is responsible for improving all local street frontages adjacent to the site regardless of whether or not access is taken to all of the adjacent streets.

Street Section and Right-of-Way Policy: District Policy 7207.5 states that right-of-way widths for all local streets shall generally not be less than 47-feet wide and that the standard street section shall be 33-feet (back-of-curb to back-of-curb).

Standard Urban Local Street—33-foot Street Section and Right-of-way Policy: District Policy 7207.5.2 states that the standard street section shall be 33-feet (back-of-curb to back-of-curb) for developments with any buildable lot that is less than 1 acre in size. This street section shall include curb, gutter, and minimum 5-foot wide concrete sidewalks on both sides and shall typically be constructed within 47-feet of right-of-way.

For the City of Kuna and City of Star: Unless otherwise approved by Kuna or Star, the standard street section shall be 36-feet (back-of-curb to back-of-curb) for developments with any buildable lot that is less than 1 acre in size. This street section shall include curb, gutter, and minimum 5-foot wide concrete sidewalks on both sides and shall typically be constructed within 50-feet of right-of-way.

Sidewalk Policy: District Policy 7207.5.7 states that five-foot wide concrete sidewalk is required on both sides of all local street, except those in rural developments with net densities of one dwelling unit per 1.0 acre or less, or in hillside conditions where there is no direct lot frontage, in which case a sidewalk shall be constructed along one side of the street. Some local jurisdictions may require wider sidewalks.

The sidewalk may be placed next to the back-of-curb. Where feasible, a parkway strip at least 8-feet wide between the back-of-curb and the street edge of the sidewalk is recommended to provide increased safety and protection of pedestrians and to allow for the planting of trees in accordance with the District's Tree Planting Policy. If no trees are to be planted in the parkway strip, the applicant may submit a request to the District, with justification, to reduce the width of the parkway strip.

Detached sidewalks are encouraged and should be parallel to the adjacent roadway. Meandering sidewalks are discouraged.

A permanent right-of-way easement shall be provided if public sidewalks are placed outside of the dedicated right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

Cul-de-sac Streets Policy: District policy 7207.5.8 requires cul-de-sacs to be constructed to provide a minimum turning radius of 50-feet; in rural areas or for temporary cul-de-sacs the emergency service providers may require a greater radius. Landscape and parking islands may be constructed in turnarounds if a minimum 29-foot street section is constructed around the island. The pavement width shall be sufficient to allow the turning around of a standard AASHTO SU design vehicle without backing. The developer shall provide written approval from the appropriate fire department for this design element.

The District will consider alternatives to the standard cul-de-sac turnaround on a case-by-case basis. This will be based on turning area, drainage, maintenance considerations and the written approval of the agency providing emergency fire service for the area where the development is located.

Landscape Medians Policy: District policy 7207.5.16 states that landscape medians are permissible where adequate pavement width is provided on each side of the median to accommodate the travel lanes and where the following is provided:

- The median is platted as right-of-way owned by ACHD.
- The width of an island near an intersection is 12-feet maximum for a minimum distance of 150-feet. Beyond the 150-feet, the island may increase to a maximum width of 30-feet.
- At an intersection that is signalized or is to be signalized in the future, the median width shall be reduced to accommodate the necessary turn lane storage and tapers.
- The Developer or Homeowners Association shall apply for a license agreement if landscaping is to be placed within these medians.
- The license agreement shall contain the District's requirements of the developer including, but not limited to, a "hold harmless" clause; requirements for maintenance by the developer; liability insurance requirements; and restrictions.

- Vertical curbs are required around the perimeter of any raised median. Gutters shall slope away from the curb to prevent ponding.

6. Roadway Offsets

Local Offset Policy: District policy 7207.4.2, requires local roadways to align or provide a minimum offset of 125-feet from any other street (measured centerline to centerline).

7. Stub Streets

Stub Street Policy: District policy 7207.2.4.3 (local) states that stub streets will be required to provide circulation or to provide access to adjoining properties. Stub streets will conform with the requirements described in 7207.2.4 (local) except a temporary cul-de-sac will not be required if the stub street has a length no greater than 150-feet. A sign shall be installed at the terminus of the stub street stating that, "THIS ROAD WILL BE EXTENDED IN THE FUTURE."

In addition, stub streets must meet the following conditions:

- A stub street shall be designed to slope towards the nearest street intersection within the proposed development and drain surface water towards that intersection; unless an alternative storm drain system is approved by the District.
- The District may require appropriate covenants guaranteeing that the stub street will remain free of obstructions.

Temporary Dead End Streets Policy: 7207.2.4.4 (local) requires that the design and construction for cul-de-sac streets shall apply to temporary dead end streets. The temporary cul-de-sac shall be paved and shall be the dimensional requirements of a standard cul-de-sac. The developer shall grant a temporary turnaround easement to the District for those portions of the cul-de-sac which extend beyond the dedicated street right-of-way. In the instance where a temporary easement extends onto a buildable lot, the entire lot shall be encumbered by the easement and identified on the plat as a non-buildable lot until the street is extended.

8. Tree Planters

Tree Planter Policy: Tree Planter Policy: The District's Tree Planter Policy prohibits all trees in planters less than 8-feet in width without the installation of root barriers. Class II trees may be allowed in planters with a minimum width of 8-feet, and Class I and Class III trees may be allowed in planters with a minimum width of 10-feet.

9. Landscaping

Landscaping Policy: A license agreement is required for all landscaping proposed within ACHD right-of-way or easement areas. Trees shall be located no closer than 10-feet from all public storm drain facilities. Landscaping should be designed to eliminate site obstructions in the vision triangle at intersections. District Policy 5104.3.1 requires a 40-foot vision triangle and a 3-foot height restriction on all landscaping located at an uncontrolled intersection and a 50-foot offset from stop signs. Landscape plans are required with the submittal of civil plans and must meet all District requirements prior to signature of the final plat and/or approval of the civil plans.

10. Pathway Crossings

United States Access Board R304.5.1.2 Shared Use Paths. In shared use paths, the width of curb ramps runs and blended transitions shall be equal to the width of the shared use path.

AASHTO's Guidelines for the Development of Bicycle Facilities 5.3.5 Other Intersection Treatments: The opening of a shared use path at the roadway should be at least the same width as the shared use path itself. If a curb ramp is provided, the ramp should be the full width of the path, not including any flared sides if utilized. . . . Detectable warnings should be placed across the full width of the ramp.

FHWA's "Designing Sidewalks and Trails for Access" (1999) reflected common ADA-related concepts: Chapter 6, Page 16-6: The width of the ramp should be at least as wide as the average width of the trail to improve safety for users who will be traveling at various speeds. In addition, the overall width of the trail should be increased, so the curb ramp can be slightly offset to the side. The increased width reduces conflict at the intersection by providing more space for users at the bottom of the ramp.

F. Standard Conditions of Approval

1. All proposed irrigation facilities shall be located outside of the ACHD right-of-way (including all easements). Any existing irrigation facilities shall be relocated outside of the ACHD right-of-way (including all easements).
2. Private Utilities including sewer or water systems are prohibited from being located within the ACHD right-of-way.
3. In accordance with District policy, 7203.3, the applicant may be required to update any existing non-compliant pedestrian improvements abutting the site to meet current Americans with Disabilities Act (ADA), Public Right-of-Way Accessibility Guidelines (PROWAG), ISPWC, or ACHD requirements. The applicant's engineer should provide documentation of compliance to District Development Review staff for review.
4. Replace any existing damaged curb, gutter and sidewalk and any that may be damaged during the construction of the proposed development. Contact Construction Services at 387-6280 (with file number) for details.
5. A license agreement and compliance with the District's Tree Planter policy is required for all landscaping proposed within ACHD right-of-way or easement areas.
6. All utility relocation costs associated with improving street frontages abutting the site shall be borne by the developer.
7. It is the responsibility of the applicant to verify all existing utilities within the right-of-way. The applicant at no cost to ACHD shall repair existing utilities damaged by the applicant. The applicant shall be required to call DIGLINE (1-811-342-1585) at least two full business days prior to breaking ground within ACHD right-of-way. The applicant shall contact ACHD Traffic Operations 387-6190 in the event any ACHD conduits (spare or filled) are compromised during any phase of construction.
8. Utility street cuts in pavement less than five years old are not allowed unless approved in writing by the District. Contact the District's Utility Coordinator at 387-6258 (with file numbers) for details.
9. All design and construction shall be in accordance with the ACHD Policy Manual, ISPWC Standards and approved supplements, Construction Services procedures and all applicable ACHD Standards unless specifically waived herein. An engineer registered in the State of Idaho shall prepare and certify all improvement plans.
10. Construction, use and property development shall be in conformance with all applicable requirements of ACHD prior to District approval for occupancy.
11. No change in the terms and conditions of this approval shall be valid unless they are in writing and signed by the applicant or the applicant's authorized representative and an authorized representative of ACHD. The burden shall be upon the applicant to obtain written confirmation of any change from ACHD.
12. If the site plan or use should change in the future, ACHD Planning Review will review the site plan and may require additional improvements to the transportation system at that time. Any change in the planned use of the property, which is the subject of this application, shall require the applicant to comply with ACHD Policy and Standard Conditions of Approval in place at that time unless a waiver/variance of the requirements or other legal relief is granted by the ACHD Commission.

G. Conclusions of Law

1. The proposed site plan is approved, if all of the Site Specific and Standard Conditions of Approval are satisfied.
2. ACHD requirements are intended to assure that the proposed use/development will not place an undue burden on the existing vehicular transportation system within the vicinity impacted by the proposed development.

Request for Appeal of Staff Decision

To request an appeal of a staff level decision, see District policy 7101.6.7 at <https://www.achdidaho.org/home/showpublisheddocument/452/638243231708370000>

Request for Reconsideration of Commission Action

To request reconsideration of a Commission Action, see District policy 1006.11 at <https://www.achdidaho.org/home/showpublisheddocument/452/638243231708370000>

RE: City of Kuna Request for Comment Case No. 25-09-S & 25-29-DR

From Stacey DuPuis <sdupuis@adacounty.id.gov>

Date Tue 9/30/2025 11:40 AM

To Marina Lundy <MLundy@kunaid.gov>

Hi Marina,
Ada County has not comment on this application.
Thank you,
Stacey



Stacey DuPuis
Community & Regional Planner
Ada County Development Services
200 W. Front St., Boise, ID 83702
(208) 287-7944 *office*
(208) 287-7909 *fax*

From: Marina Lundy <MLundy@kunaid.gov>

Sent: Monday, September 29, 2025 2:26 PM

To: Ada County Engineer <agilman@adaweb.net>; Ada County Highway District <planningreview@achdidaho.org>; Adam Ingram <adam.ingram@cableone.biz>; Angel Robins (Idaho Power) <ARobins@idahopower.com>; Bobby Withrow <bwithrow@kunaid.gov>; Boise Kuna Irrigation District <aflavel.bkirrdist@gmail.com>; Boise Project Board of Control <tritthaler@boiseproject.org>; Boise Project Board of Control 2 <gashley@boiseproject.org>; Brent Moore <bmoore@adacounty.id.gov>; Brian Graves Kuna School District <bgraves@kunaschools.org>; Camille Burt (USPS) <camille.r.burt@usps.gov>; Central District Health Department <lbadigian@cdhd.idaho.gov>; Mike Fratusco <mfratusco@adacounty.id.gov>; COMPASS <gisshared@compassidaho.org>; David Reinhardt <reinhardt.david@westada.org>; DEQ <BRO.Admin@deq.idaho.gov>; Eric Adolfson <eadolfson@compassidaho.org>; Erika Olvera (NMID) <eolvera@nmid.org>; Idaho Power (Taylor Dunn) <tdunn@idahopower.com>; Idaho Power Easements <easements@idahopower.com>; Idaho Power Easements 2 <kfunke@idahopower.com>; Intermountain Gas <bryce.ostler@intgas.com>; ITD <D3Development.Services@itd.idaho.gov>; ITD Kendra Conder <Kendra.Conder@itd.idaho.gov>; J&M Sanitation <Chad.Gordon@jmsanitation.com>; Jason Reddy (KSD) <jjreddy@kunaschools.org>; Jonathon Gillen <gillen.jonathon@westada.org>; Justin Walker <jwalker@kellerassociates.com>; Sam Feist <SFeist@kunaid.gov>; Kuna Fire Office <office@kunafire.com>; lletson@adacounty.id.gov; Lumen - Ebin Barnett <ebin.barnett@lumen.com>; Marc Boyer (Kuna Postmaster) <marc.c.boyer@usps.gov>; Megan Leatherman <mleatherman@adaweb.net>; Meridian Fire (Brandon Medica) <bmedica@meridiancity.org>; Meridian Fire (Steve Taublee) <staulbee@meridiancity.org>; Michelle Covert <mcovert@kunaid.gov>; Nampa Meridian Irrigation District <nmid@nmid.org>; New York Irrigation District <nyirrigation@nyid.org>; Niki Benyakhlef ITD <Niki.Benyakhlef@itd.idaho.gov>; Paris Dickerson <PDickerson@idahopower.com>; PWoffice <PWoffice@kunaid.gov>; Robbie Reno <rreno@kunaschools.org>; Scott Arellano (KRFD) <scott@fccnw.com>; Sparklight/Cable One (John Walburn) <john.walburn@cableone.biz>; Stacey DuPuis <sdupuis@adacounty.id.gov>; Tim Jensen (Kuna School District) <tejensen@kunaschools.org>; TJ Lawrence <tlawrence@kunafire.com>

Cc: Troy Behunin <tbehunin@kunaid.gov>; Taryn Villanueva <TVillanueva@kunaid.gov>

Subject: [EXTERNAL] City of Kuna Request for Comment Case No. 25-09-S & 25-29-DR

CAUTION: This email originated from outside Ada County email servers. Do not click on links or open attachments unless you recognize the sender and know the content is safe. Verify the sender by mouse-hovering over their display name in order to see the sender's full email address and confirm it is not suspicious. If you are unsure an email is safe, please report the email by using the 'Phish Alert' button in Outlook.

Good afternoon,

Notice is hereby given by the City of Kuna the following actions are under consideration:

CASE NUMBER:	INDIE SUBDIVISION 25-09-S & 25-29-DR
PROJECT DESCRIPTION	Applicant requests Subdivision and Design Review approval for a 23-lot single family residential subdivision. The site is located near the intersection of W Ardell Rd. And S McClure Ln. (APN: S1315427820)
APPLICANT	Tamee Crawford Tlcrawford@centengr.com 208.343.3381
REPRESENTATIVE	David Crawford Dacrawford@centengr.com 208.343.3381
SCHEDULED HEARING DATE	Tuesday, November 25, 2025 , at 6:00 P.M.
STAFF CONTACT	Marina Lundy 986.269.8341 mlundy@kunaid.gov
<ul style="list-style-type: none">· We have enclosed information to assist you with your consideration and response; we would appreciate any information as to how this action would affect the service(s) your agency provides. <i>If your agency requires additional information, or if contact information for your agency needs updated, please notify our office.</i>· <i>No response within 15 business days will indicate you have no objection or comments regarding this project.</i>· The hearing is scheduled to begin at 6:00 p.m. or as soon as it may be heard. Kuna City Hall is located at 751 W. 4th Street, Kuna, ID 83634. Please contact staff with questions.	



Marina Lundy

Planner

City of Kuna | Development Services

751 W. 4th Street

P.O. Box 13

Kuna, ID 83634

Phone: 986.269.8341

Email: mlundy@kunaid.gov
www.kunacity.id.gov

RECEIVED

OCT 10 2025

CITY OF KUNA

Boise-Kuna Irrigation District

129 N. School Avenue
Kuna, ID 83634-2201

David Reynolds, Director
Richard Durrant, Director
Daniel Sheirbon, Director

Telephone (208) 922-5608
Fax (208) 922-5659

October 7, 2025

City of Kuna
ATTN: Marina Lundy, Planner
751 W. 4th Street
Kuna, Idaho 83634

BKID Account: BK 287B1
RE: Indie Subdivision 25-09-S, 25-29-DR
Parcel: S1315427820
W. Ardell Road and S. McClure Lane, Kuna, Idaho 83634

Dear Ms. Lundy:

Boise-Kuna Irrigation District would like to reiterate the Boise Project Board of Control letter referring to Idaho Statutes, Title 42, local irrigation/drainage ditches that cross this property, to serve neighboring properties, must remain unobstructed and protected by an appropriate easement by the landowner, developer, contractors.

We would like to see the irrigation deliveries and drainage ditches to adjoining properties identified on a GIS map. Our purpose is to ensure our patrons have use of their water as they have historically and that no harm is being incurred from the newly proposed development.

We would also like to make sure that all the easements are abided by in the letter from Boise Project.

If you have any questions, please feel to contact our office at 208-922-5608.

Sincerely,

David Reynolds
Chairman

cc: David Crawford: Centurion Engineers

RICHARD MURGOITIO
CHAIRMAN OF THE BOARD

DAN SHEIRBON
VICE CHAIRMAN OF THE BOARD

ROBERT D. CARTER
PROJECT MANAGER

THOMAS RITTHALER
ASSISTANT PROJECT MANAGER

APRYL GARDNER
SECRETARY-TREASURER

MARY SUE CHASE
ASSISTANT SECRETARY-
TREASURER

BOISE PROJECT BOARD OF CONTROL

(FORMERLY BOISE U.S. RECLAMATION PROJECT)

2465 OVERLAND ROAD
BOISE, IDAHO 83705-3155

RECEIVED
OCT 07 2025
CITY OF KUNA

OPERATING AGENCY FOR 167,000
ACRES FOR THE FOLLOWING
IRRIGATION DISTRICTS

NAMPA-MERIDIAN DISTRICT
BOISE-KUNA DISTRICT
WILDER DISTRICT
NEW YORK DISTRICT
BIG BEND DISTRICT

TEL: (208) 344-1141
FAX: (208) 344-1437

02 October 2025

City of Kuna
751 W. 4th St.
Kuna, Idaho 83634

RE: Mark Kirkpatrick, Indie Sub
2021 W Ardell Rd Kuna 83634
Boise-Kuna Irrigation District
Teed Lateral 281+90
Sec. 15, T2N, R1W, BM.

25-09-S, 25-29-DR

BK-287 B

Marina Lundy Planner:

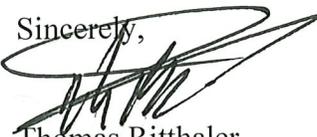
There are no Boise Project or Boise-Kuna Irrigation District facilities located on the above-mentioned property, however it does in fact possess a valid water right.

Per Idaho Statutes, Title 42, local irrigation/drainage ditches that cross this property, to serve neighboring properties, must remain unobstructed and protected by an appropriate easement by the landowner, developer and contractors. Landowner/developer must do their due diligence to contact all owners of neighboring properties on this matter.

This development is subject to Idaho Code 31-3805, in accordance, this office is requesting a full-size hard copy of the irrigation and drainage plans.

If you have any further questions or comments regarding this matter, please do not hesitate to contact me at (208) 344-1141.

Sincerely,


Thomas Ritthaler
Assistant Project Manager, BPBC

tbr/tr

cc: Kevin Reeves
Alicia Flavel

Watermaster, Div; 2 BPBC
Secretary – Treasurer, BKID

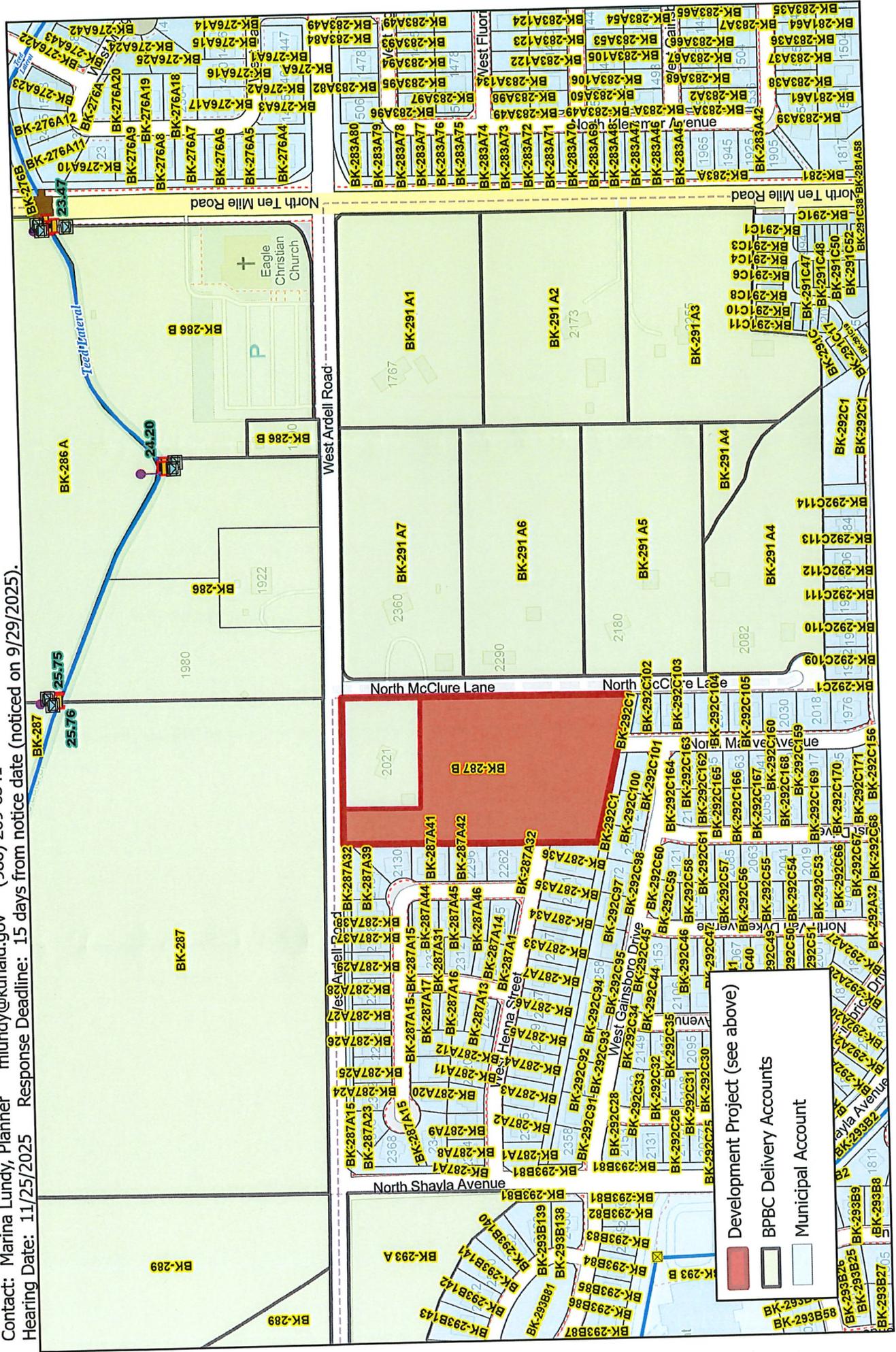
Case # 25-09-S & 25-29-DR Preliminary Plat & Design Review - Indie Subdivision 2021 W Ardell Rd, Kuna

BK-287 B KIRKPATRICK, MARK TEED LATERAL, TAP 24.22 6.80 WATER RIGHT ACRES

NOTE: Property has split into 2 parcels, development parcel is the shaded area.

Agency: City of Kuna Development Services
 Contact: Marina Lundy, Planner mlundy@kunaid.gov (986) 269-8341
 Hearing Date: 11/25/2025 Response Deadline: 15 days from notice date (noticed on 9/29/2025).

X: Scans\IndieSubdivision





Ada County Transmittal
Division of Community and Environmental Health

Return to:

- ACZ
- Boise
- Eagle
- Garden City
- Meridian
- Kuna
- Star

Rezone/OTD # _____

Conditional/Accessory Use # _____

Preliminary / Final / Short Plat 25-09-S + 25-29-DR

Development Name/Section Indie CDH File # _____

- 1. We have No Objections to this Proposal.
- 2. We recommend Denial of this Proposal.
- 3. Specific knowledge as to the exact type of use must be provided before we can comment on this Proposal.
- 4. Before we can comment concerning individual sewage disposal, we will require more data concerning the depth of:
 - high seasonal ground water
 - bedrock from original grade
 - waste flow characteristics
 - other _____
- 5. This office may require a study to assess the impact of nutrients and pathogens to receiving ground waters and surface waters.
- 6. After written approvals from appropriate entities are submitted, we can approve this proposal for:
 - central sewage
 - interim sewage
 - individual sewage
 - community sewage system
 - central water
 - individual water
 - community water well
- 7. The following plan(s) must be submitted to and approved by the Idaho Department of Environmental Quality:
 - central sewage
 - sewage dry lines
 - community sewage system
 - central water
 - community water
- 8. Infiltration beds for storm water disposal are considered shallow injection wells. If they are not in the City of Boise or ACHD right-of-way, an application and fee per well, vicinity map and construction plans must be submitted to CDH.
- 9. If restroom or plumbing facilities are to be installed, then a sewage system MUST be installed to meet Idaho State Sewage Regulations.
- 10. An accessory use application, fee, detailed site plan and floor plans must be submitted to CDH for review.
- 11. Land development application, fee per lot, test holes and full engineering report is required.
- 12. CDH makes no guarantee a septic permit will be issued on the split off lot. A speculative site evaluation is recommended.
- 13. We will require plans be submitted for a plan review for any:
 - food establishment
 - beverage establishment
 - swimming pools or spas
 - grocery store
 - child care center
- 14. _____

Reviewed By: Row Barry Date: 10/14/25



October 6, 2025

Marina Lundy, Planner
City of Kuna
751 W. 4th Street
Kuna, ID 83634
mlundy@kunaid.gov

Subject: City of Kuna Request for Comment Case No. 25-09-S & 25-29-DR

Dear Ms. Lundy:

Thank you for the opportunity to respond to your request for comment. While DEQ does not review every project on a project-specific basis, we attempt to provide the best review of the information provided. DEQ encourages agencies to review and utilize the Idaho Environmental Guide to assist in addressing project-specific conditions that may apply. This guide can be found at: <https://www2.deq.idaho.gov/admin/LEIA/api/document/download/15083>.

The following information does not cover every aspect of this project; however, we have the following general comments to use as appropriate:

1. AIR QUALITY

- Please review IDAPA 58.01.01 for all rules on Air Quality, especially those regarding fugitive dust (58.01.01.651), and trade waste burning (58.01.01.600-617).
- For new development projects, all property owners, developers, and their contractors must ensure that reasonable controls to prevent fugitive dust from becoming airborne are utilized during all phases of construction activities per IDAPA 58.01.01.651.
- DEQ recommends the city/county require the development and submittal of a dust prevention and control plan for all construction projects prior to final plat approval. Dust prevention and control plans incorporate appropriate best management practices to control fugitive dust that may be generated at sites.
- Citizen complaints received by DEQ regarding fugitive dust from development and construction activities approved by cities or counties will be referred to the city/county to address under their ordinances.
- Per IDAPA 58.01.01.600-617, the open burning of any construction waste is prohibited. The property owner, developer, and their contractors are responsible for ensuring no prohibited open burning occurs during construction.
- For questions, contact David Luft, Air Quality Manager, at (208) 373-0550.

2. WASTEWATER AND RECYCLED WATER

- DEQ recommends verifying that there is adequate sewer to serve this project prior to approval. Please contact the sewer provider for a capacity statement, declining balance report, and willingness to serve this project.
- IDAPA 58.01.16 and IDAPA 58.01.17 are the sections of Idaho rules regarding wastewater and recycled water. Please review these rules to determine whether this or future projects will require DEQ approval. IDAPA 58.01.03 is the section of Idaho rules regarding subsurface disposal of wastewater. Please review this rule to determine whether this or future projects will require permitting by the local public health district.
- All projects for construction or modification of wastewater systems require preconstruction approval. Recycled water projects and subsurface disposal projects require separate permits as well.
- DEQ recommends that projects be served by existing approved wastewater collection systems or a centralized community wastewater system whenever possible. Please contact DEQ to discuss potential for development of a community treatment system along with best management practices for communities to protect groundwater.
- DEQ recommends that cities and counties develop and use a comprehensive land use management plan, which includes the impacts of present and future wastewater management in this area. Please schedule a meeting with DEQ for further discussion and recommendations for plan development and implementation.

For questions, contact Valerie Greear, Water Quality Engineering Manager at (208) 373-0550.

3. DRINKING WATER

- DEQ recommends verifying that there is adequate water to serve this project prior to approval. Please contact the water provider for a capacity statement, declining balance report, and willingness to serve this project.
- DEQ recommends verifying if the current and/or proposed drinking water system is a regulated public drinking water system. A drinking water system is a Public Water System (PWS) if it has at least 15 service connections or regularly serves an average of 25 or more people per day for at least 60 days per year (refer to the DEQ website at: <https://www.deq.idaho.gov/water-quality/drinking-water/>). For non-regulated systems, DEQ recommends annual testing for total coliform bacteria, nitrate, and nitrite.
- IDAPA 58.01.08 is the section of Idaho rules regarding public drinking water systems. Please review these rules to determine whether this or future projects will require DEQ approval.
- All projects for construction or modification of public drinking water systems require preconstruction approval.
- If any private wells will be included in this project, we recommend that they be tested for total coliform bacteria, nitrate, and nitrite prior to use and retested annually thereafter.
- DEQ recommends using an existing drinking water system whenever possible or construction of a new community drinking water system. Please contact DEQ to discuss this project and to explore options to both best serve the future residents of this development and provide for protection of groundwater resources.
- DEQ recommends cities and counties develop and use a comprehensive land use management plan which addresses the present and future needs of this area for adequate, safe, and sustainable drinking water. Please schedule a meeting with DEQ for further discussion and recommendations for plan development and implementation.

For questions, contact Valerie Greear, Water Quality Engineering Manager at (208) 373-0550.

4. SURFACE WATER

- Please contact DEQ to determine whether this project will require an Idaho Pollutant Discharge Elimination System (IPDES) Permit. A Multi-Sector General Permit from DEQ may be required for facilities that have an allowable discharge of storm water or authorized non-storm water associated with the primary industrial activity and co-located industrial activity. For questions, contact Emily Montague, IPDES Compliance Supervisor, at (208) 373-0433.
- If this project is near a source of surface water, DEQ requests that projects incorporate construction best management practices (BMPs) to assist in the protection of Idaho's water resources. Additionally, please contact DEQ to identify BMP alternatives and to determine whether this project is in an area with Total Maximum Daily Load stormwater permit conditions.
- The Idaho Stream Channel Protection Act requires a permit for most stream channel alterations. Please contact the Idaho Department of Water Resources (IDWR), Western Regional Office, at 2735 Airport Way, Boise, or call (208) 334-2190 for more information. Information is also available on the IDWR website at: <https://idwr.idaho.gov/streams/stream-channel-alteration-permits.html>
- The Federal Clean Water Act requires a permit for filling or dredging in waters of the United States. Please contact the US Army Corps of Engineers, Boise Field Office, at 10095 Emerald Street, Boise, or call 208-345-2155 for more information regarding permits.

For questions, contact Lance Holloway, Surface Water Manager, at (208) 373-0550.

5. SOLID WASTE, HAZARDOUS WASTE AND GROUNDWATER CONTAMINATION

- **Solid Waste.** No trash or other solid waste shall be buried, burned, or otherwise disposed of at the project site. These disposal methods are regulated by various state regulations including Idaho's Solid Waste Management Regulations and Standards (IDAPA 58.01.06), Rules and Regulations for Hazardous Waste (IDAPA 58.01.05), and Rules and Regulations for the Prevention of Air Pollution (IDAPA 58.01.01). Inert and other approved materials are also defined in the Solid Waste Management Regulations and Standards.
- **Hazardous Waste.** The types and number of requirements that must be complied with under the federal Resource Conservation and Recovery Act (RCRA) and the Idaho Rules and Standards for Hazardous Waste (IDAPA 58.01.05) are based on the quantity and type of waste generated. Every business in Idaho is required to track the volume of waste generated, determine whether each type of waste is hazardous, and ensure that all wastes are properly disposed of according to federal, state, and local requirements.
- **Water Quality Standards.** Site activities must comply with the Idaho Water Quality Standards (IDAPA 58.01.02) regarding hazardous and deleterious-materials storage, disposal, or accumulation adjacent to or in the immediate vicinity of state waters (IDAPA 58.01.02.800); and the cleanup and reporting of oil-filled electrical equipment (IDAPA 58.01.02.849); hazardous materials (IDAPA 58.01.02.850); and used-oil and petroleum releases (IDAPA 58.01.24.060 and 58.01.24.061). Petroleum releases must be reported to DEQ in accordance with IDAPA 58.01.24.060.01 and 58.01.24.061.04. Hazardous material releases to state waters, or to land such that there is likelihood that it will enter state waters, must be reported to DEQ in accordance with IDAPA 58.01.02.850.

- **Groundwater Contamination.** DEQ requests that this project comply with Idaho's Ground Water Quality Rules (IDAPA 58.01.11), which states that "No person shall cause or allow the release, spilling, leaking, emission, discharge, escape, leaching, or disposal of a contaminant into the environment in a manner that causes a ground water quality standard to be exceeded, injures a beneficial use of ground water, or is not in accordance with a permit, consent order or applicable best management practice, best available method or best practical method."

For questions, contact Matthew Pabich, Waste & Remediation Manager, at (208) 373-0550.

6. ADDITIONAL NOTES

- If an underground storage tank (UST) or an aboveground storage tank (AST) is identified at the site, additional regulations may apply. If an UST is present, the site should be evaluated to determine whether the UST is regulated by DEQ. If an AST is identified, EPA may have additional requirements. Both UST and AST sites should be assessed to determine whether there is potential soil and ground water contamination. Please call DEQ at (208) 373-0550, or visit the DEQ website <https://www.deq.idaho.gov/waste-management-and-remediation/storage-tanks/leaking-underground-storage-tanks-in-idaho/> for assistance. If applicable to this project, DEQ recommends that BMPs be implemented for any of the following land uses: wash water from cleaning vehicles, fertilizers and pesticides, animal facilities, composted waste, ponds and outdoor gun ranges. Please contact DEQ for more information on any of these conditions.

We look forward to working with you in a proactive manner to address potential environmental impacts that may be within our regulatory authority. If you have any questions, please contact me, or any of our technical staff at (208) 373-0550.

Sincerely,



Troy Smith
Regional Administrator

RE: City of Kuna Request for Comment Case No. 25-09-S & 25-29-DR

From Kendra Conder <Kendra.Conder@itd.idaho.gov>

Date Tue 9/30/2025 3:07 PM

To Marina Lundy <MLundy@kunaid.gov>

Hi Marina,

ITD has reviewed the application transmittal for 25-09-S & 25-29-DR and does not have any comments.

Thank you!

Kendra Conder

District 3 | Development Services Coordinator

Idaho Transportation Department

Office: 208-334-8377

Cell: 208-972-3190



From: Marina Lundy <MLundy@kunaid.gov>

Sent: Monday, September 29, 2025 2:26 PM

To: Ada County Engineer <agilman@adaweb.net>; Ada County Highway District <planningreview@achdidaho.org>; Adam Ingram <adam.ingram@cableone.biz>; Angel Robins (Idaho Power) <ARobins@idahopower.com>; Bobby Withrow <bwithrow@kunaid.gov>; Boise Kuna Irrigation District <aflavel.bkirrdist@gmail.com>; Boise Project Board of Control <tritthaler@boiseproject.org>; Boise Project Board of Control 2 <gashley@boiseproject.org>; bmoore@adacounty.id.gov; Brian Graves Kuna School District <bgraves@kunaschools.org>; Camille Burt (USPS) <camille.r.burt@usps.gov>; Central District Health Department <lbadigian@cdhd.idaho.gov>; Chief Fratusco <mfratusco@adacounty.id.gov>; COMPASS <gisshared@compassidaho.org>; David Reinhardt <reinhardt.david@westada.org>; DEQ <BRO.Admin@deq.idaho.gov>; Eric Adolfson <eadolfson@compassidaho.org>; Erika Olvera (NMID) <eolvera@nmid.org>; Idaho Power (Taylor Dunn) <tdunn@idahopower.com>; Idaho Power Easements <easements@idahopower.com>; Idaho Power Easements 2 <kfunke@idahopower.com>; Intermountain Gas <bryce.ostler@intgas.com>; D3 Development Services <D3Development.Services@itd.idaho.gov>; Kendra Conder <Kendra.Conder@itd.idaho.gov>; J&M Sanitation <Chad.Gordon@jmsanitation.com>; Jason Reddy (KSD) <jjreddy@kunaschools.org>; Jonathon Gillen <gillen.jonathon@westada.org>; Justin Walker <jwalker@kellerassociates.com>; Sam Feist <SFeist@kunaid.gov>; Kuna Fire Office <office@kunafire.com>; lletson@adacounty.id.gov; Lumen - Ebin Barnett <ebin.barnett@lumen.com>; Marc Boyer (Kuna Postmaster) <marc.c.boyer@usps.gov>; Megan Leatherman <mleatherman@adaweb.net>; Meridian Fire (Brandon Medica) <bmedica@meridiancity.org>; Meridian Fire (Steve Taublee) <staulbee@meridiancity.org>; Michelle Covert <mcovert@kunaid.gov>; Nampa Meridian Irrigation District <nmid@nmid.org>; New York Irrigation District <nyirrigation@nyid.org>; Niki Benyakhlef <Niki.Benyakhlef@itd.idaho.gov>; Paris Dickerson <PDickerson@idahopower.com>; PWorkoffice <PWorkoffice@kunaid.gov>; Robbie Reno <rreno@kunaschools.org>; Scott Arellano (KRFD) <scott@fccnw.com>; Sparklight/Cable One (John Walburn) <john.walburn@cableone.biz>; syarrington@adacounty.id.gov; Tim Jensen (Kuna School District) <tejensen@kunaschools.org>; TLawrence Kuna Fire <tlawrence@kunafire.com>

Cc: Troy Behunin <tbehunin@kunaid.gov>; Taryn Villanueva <TVillanueva@kunaid.gov>

Subject: City of Kuna Request for Comment Case No. 25-09-S & 25-29-DR

CAUTION: This email originated outside the State of Idaho network. Verify links and attachments BEFORE you click or open, even if you recognize and/or trust the sender. Contact your agency service desk with any concerns.

Good afternoon,

Notice is hereby given by the City of Kuna the following actions are under consideration:

CASE NUMBER:	INDIE SUBDIVISION 25-09-S & 25-29-DR
PROJECT DESCRIPTION	Applicant requests Subdivision and Design Review approval for a 23-lot single family residential subdivision. The site is located near the intersection of W Ardell Rd. And S McClure Ln. (APN: S1315427820)
APPLICANT	Tamee Crawford Tlcrawford@centengr.com 208.343.3381
REPRESENTATIVE	David Crawford Dacrawford@centengr.com 208.343.3381
SCHEDULED HEARING DATE	Tuesday, November 25, 2025 , at 6:00 P.M.
STAFF CONTACT	Marina Lundy 986.269.8341 mlundy@kunaid.gov
<ul style="list-style-type: none">· We have enclosed information to assist you with your consideration and response; we would appreciate any information as to how this action would affect the service(s) your agency provides. <i>If your agency requires additional information, or if contact information for your agency needs updated, please notify our office.</i>· <i>No response within 15 business days will indicate you have no objection or comments regarding this project.</i>· The hearing is scheduled to begin at 6:00 p.m. or as soon as it may be heard. Kuna City Hall is located at 751 W. 4th Street, Kuna, ID 83634. Please contact staff with questions.	



Marina Lundy

Planner

City of Kuna | Development Services

751 W. 4th Street

P.O. Box 13

Kuna, ID 83634

Phone: 986.269.8341

Email: mlundy@kunaid.gov
www.kunacity.id.gov



CITY OF KUNA
P.O. BOX 13
KUNA, ID 83634
www.kunacity.id.gov

Michael L. Borzick
GIS Manager &
Plan Review Manager

PRELIMINARY PLAT MEMORANDUM

To: Marina Lundy - Planning and Zoning
From: Michael Borzick - GIS Manager & Plan Review Manager
Date: 30 October 2025
RE: Public Works Comments

The Indio Subdivision, is located on southwest of corner of W Ardell Rd and N McClure Ln. This review is based on land use as allowed or permitted in "R-6" zones. This application encompasses 5.249 acres. This application contains a total of 23 single-family residential lots, and 5 common lots. These comments apply to the application as they affect public works infrastructure. The applicant provided a preliminary plat and supporting documents as part of the application. Review of civil design drawings is accomplished separately, when received.

Comments may be expanded or refined in connection with the future land-use actions.

1) Inspection Fees

- a) An inspection fee will apply to inspect the final construction of water, sewer, and irrigation facilities associated with this development.
- b) The developer shall retain a qualified responsible, Idaho registered professional engineer to provide sufficient inspection to certify to DEQ that the project was completed in accordance with approved plans and specifications and to provide accurate as-built drawings to the City.
- c) The developer's engineer and the City's inspector are permitted to coordinate inspections. The current inspection fee is \$1.70 per lineal foot of sewer, water, and pressure irrigation pipe. *Payment is due and payable prior to the pre-construction meeting.*
- d) The Kuna Rural Fire District's current hydrant flow testing and plat base fee are \$300.00 and \$75.00 respectively for a total of \$375.00. *Payment is due and payable prior to the pre-construction meeting.*

2) General

- a) The Indio Subdivision residential subdivision consists of 5.249 acres within the city's designated medium density residential (R-6) zoning area.
- b) Equivalent Dwelling Units (EDUs) are rated at approximately 3.18 people per household. The resultant projected population for this subdivision is approximately 73. This makes the realized density approximately 14 people per acre.
- c) The preliminary plat shows two (2) public access points to the subdivision – one (1) off of W Henna St, and one (1) onto N Mauve Ave.

- d) Areas for outside activities are incorporated into the project. Connection to the City of Kuna pathways presents a long-term goal that should be considered.
- e) A plan approval letter will be required if this project affects any local irrigation districts.
- f) Elevations shall be actual NAVD 88 datum elevations. A localized elevation system is not acceptable.
- g) All positional information shall be from the most recent state plane coordinate system.
- h) Provide engineering certification on all final engineering drawings.
- i) The City of Kuna requires streetlights in all subdivisions. Streetlights are required along arterial roads bordering the subdivision, at the entrance of the subdivision, at intersections, and at every 250' interval. Streetlights should coincide with Fire Hydrants whenever possible.
- j) Kuna Rural Fire District (KRFD) requires fire hydrants at 500' intervals.

3) Right-of-Way

- a) Sufficient right-of-way for existing and future classified streets shall be provided pursuant to City & ACHD standards.
- b) Approaches onto classified streets must comply with ACHD approach policies.
- c) All street construction must meet or exceed City of Kuna and ACHD development standards.
- d) All City mainlines crossing proposed lots or located on the backs or sides of lots shall have easements that allow the City of Kuna to access and maintain the utilities.
- e) The KRFD Deputy Fire Marshal must approve fire access to the subdivision and any traffic calming measures.
- f) Roads must continue to and through to the next road connection to promote connectivity throughout the City. Kuna Public Works does not condone or need the eastern access onto N McClure Ln.

4) Sanitary Sewer Connection

- a) The developer may be requested to participate in sewer gravity main upgrades.
- b) The applicant's property is not connected to City services and is subject to connection fees for the ultimate connected sewer load. City code 5-16-3-B.2 states public sewer utilities shall be extended to each parcel when sewer is available within three hundred (300) feet of the parcels.
- c) There is gravity sewer pipe in both W Henna St and N Mauve Ave. The sewage flows to Crimson Lift Station, which currently has sufficient capacity.
- d) All sewer infrastructure must meet or exceed City of Kuna requirements.
- e) Sewer flow models will be required to verify pipe sizes.
- f) Sewer connection fees apply to each home or dwelling. 1 home = 1 sewer connection. 1 duplex = 2 sewer connections. 4-plex = 4 sewer connections.**
- g) All existing sewage treatment facilities (septic tank and drain field) must be decommissioned in accordance with Idaho Department of Environmental Quality requirements. Documentation shall be provided to the City of Kuna.
- h) This application shall conform to the sewer masterplan as applicable. The sewer master plan specifies minimum pipe sizes and supports the "to and through" utility policy.

5) Potable Water Connection

- a) The applicant's property is not connected to City services and is subject to connection fees for the ultimate connected water demand. City Code 6-4-2-X requires PUDs and master planned communities to have a master utility plan that addresses potable water issues. City code 5-16-3-B.2 states public water utilities shall be extended to each parcel when water is available within three hundred (300) feet of the parcels.
- b) There is water main in W Henna St (8-in.), and within N Mauve Ave (10-in.) that will need to be connected to.
- c) All water infrastructure must meet or exceed City of Kuna requirements.
- d) Water flow models will be required to verify adequate water supply and fire suppression.
- e) **Water connection fees apply to each home or dwelling. 1 home = 1 water connection. 1 duplex = 2 water connections. 4-plex = 4 water connections.**
- f) All existing wells shall be abandoned in accordance with Idaho Department of Water Resources (IDWR) requirements. Documentation shall be provided to the City of Kuna.
- g) This application shall conform to the water master plan as applicable.
- h) Fire hydrants are required in a layout acceptable to the KRFD.

6) Pressurized Irrigation

- a) The applicant's property is not connected to the City's pressurized irrigation system. Relying on drinking water for irrigation purposes is contrary to City Code 6-4-2-1.
- b) All pressurized irrigation infrastructure shall meet or exceed City of Kuna standards.
- c) This project requires connection to the City's Pressurized Irrigation system.
- d) Adequate connection to existing PI mains is required with proper valving.
- e) Annexation into the municipal irrigation district and pooling of all applicable water rights is a requirement of the final plat approval. If adequate water rights to cover said development are not available we will need to discuss if this project can go forward.
- f) Existing irrigation ditches (supply & drain) must be relocated as needed and as approved by the irrigation ditch company/users.
- g) Pressurized irrigation flow models will be required to verify adequate pressurized irrigation supply.
- h) All residential, common lots, and open areas with irrigation are required to connect to the pressurized irrigation system and to pay the associated connection fee.
- i) This application shall be conditioned to conform to the Pressure Irrigation Master Plan where applicable.

7) Grading and Storm Drainage

- a) Provide a grading and drainage plan which supports and maintains all upstream drainage rights and all downstream irrigation delivery rights as they presently exist for this property.
- b) The City of Kuna relies on the ACHD Stormwater Policy Manual to establish the requirements for design of private storm water disposal systems.
- c) Runoff from public right-of-way is regulated by ACHD. On site storm water retention shall be reviewed in conjunction with the City's Civil Engineering Construction Improvements Review. Provide a storm water disposal & treatment plan which accounts for increased on-site storm water runoff volumes. Provided detailed drawings of drainage & treatment facilities with supporting calculation for review and approval.

- d) Sidewalks, curb and gutter, street widening and any related storm drainage facilities, consistent with city code and policies, shall be provided in connection with property development.
- e) Verify that existing and proposed elevations match property boundaries such that a slope burden is not imposed on adjacent properties. Slopes shall not be steeper than 3:1 on lots adjacent to a street or common lot and no steeper than 4:1 for lots with common rear lot lines.

8) As-Built Drawings

- a) As-built drawings are required at the conclusion of any public facility construction project and are the responsibility of the developer's engineer. The city may help track changes but will not be responsible for the finished product. As-built drawings will be required before occupancy or final plat approval is granted.

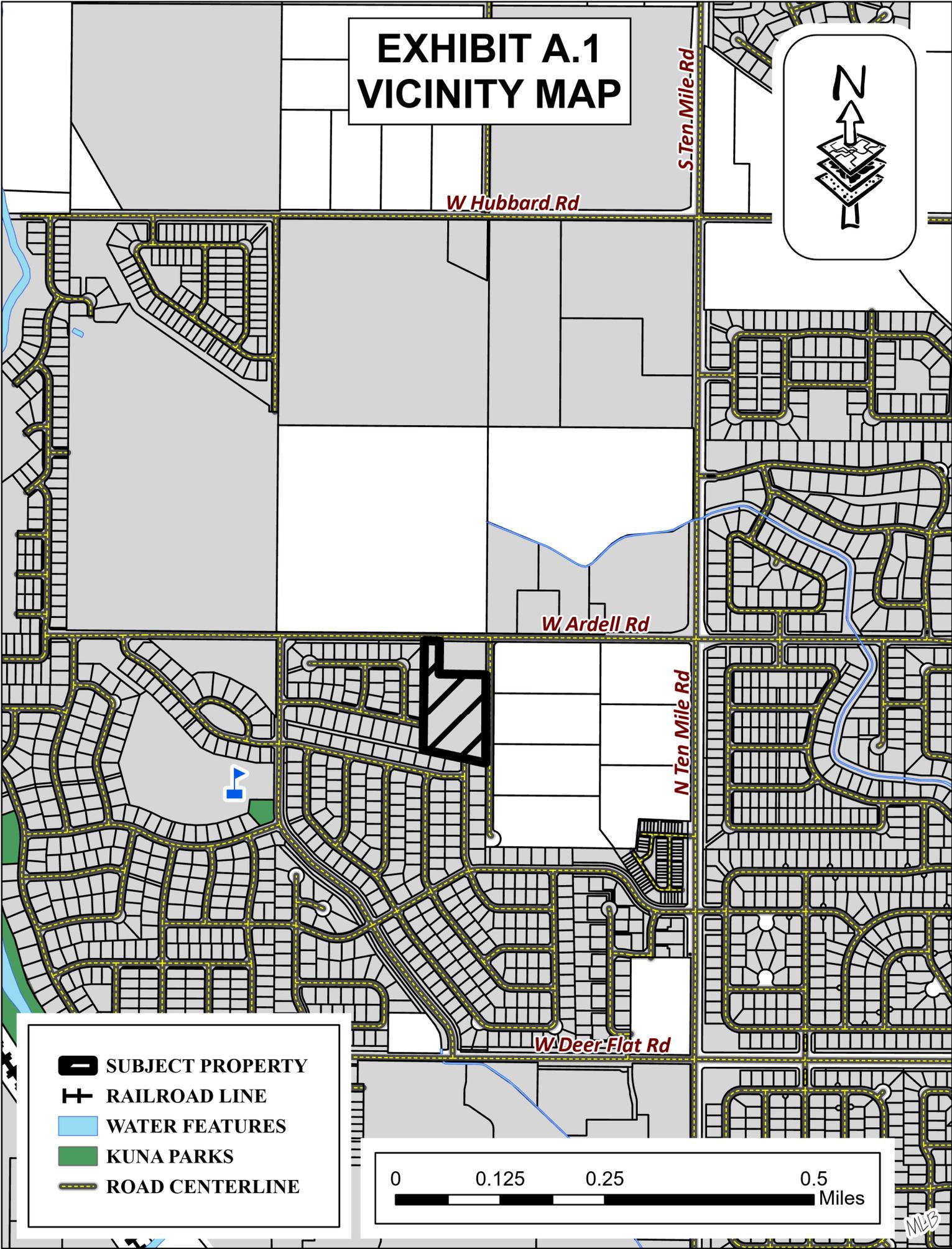
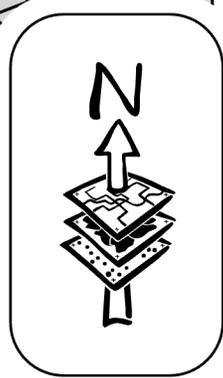
9) Recommendation

Public Works can fully support this infill type area be constructed into a residential area as designed; with some minor alterations to the Public Works infrastructure plan. The connection to N McClure Ln is not needed by the Public Works Department for utilities at the moment, but if there is a need to get water or sewer into N McClure Ln we may need an HOA style lot, path lot or an easement.

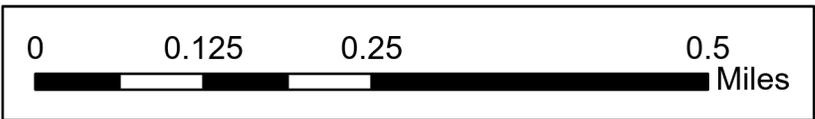
10) Exhibits

- a) Exhibit A.1 – Vicinity Map
- b) Exhibit A.2 – Topo Map
- c) Exhibit A.3 – Lot Value Map
- d) Exhibit A.4 – Lot Size Map
- e) Exhibit A.5 – Soil Slope Map
- f) Exhibit A.6 – Comp Plan Map
- g) Exhibit A.7 – Pathway and Bike Lane Map

EXHIBIT A.1 VICINITY MAP

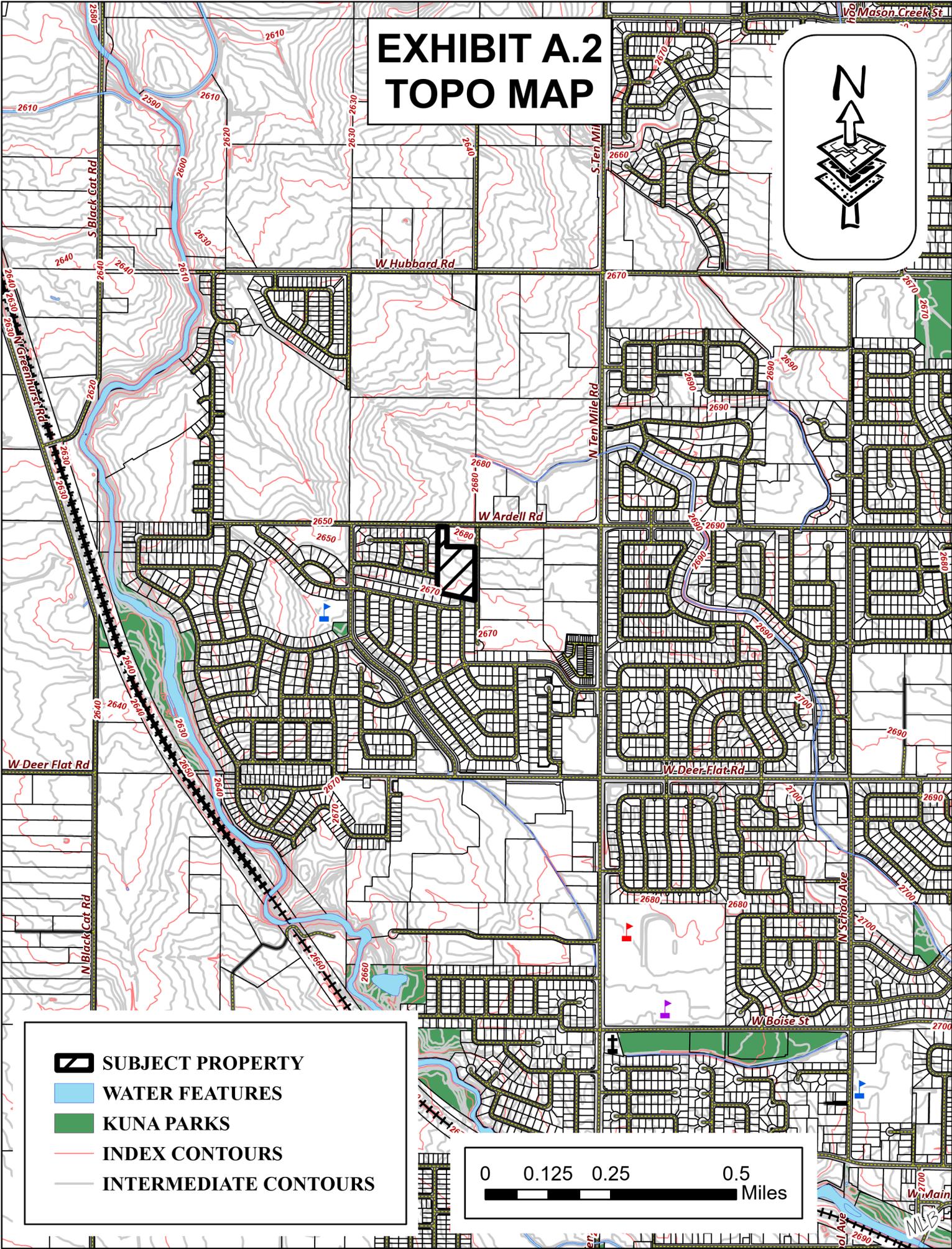
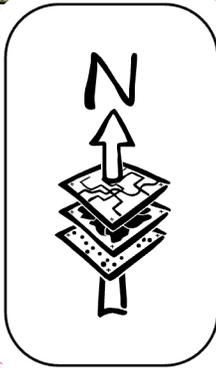


-  SUBJECT PROPERTY
-  RAILROAD LINE
-  WATER FEATURES
-  KUNA PARKS
-  ROAD CENTERLINE



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EXHIBIT A.2 TOPO MAP



-  SUBJECT PROPERTY
-  WATER FEATURES
-  KUNA PARKS
-  INDEX CONTOURS
-  INTERMEDIATE CONTOURS

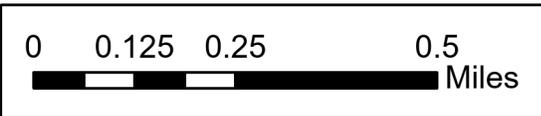
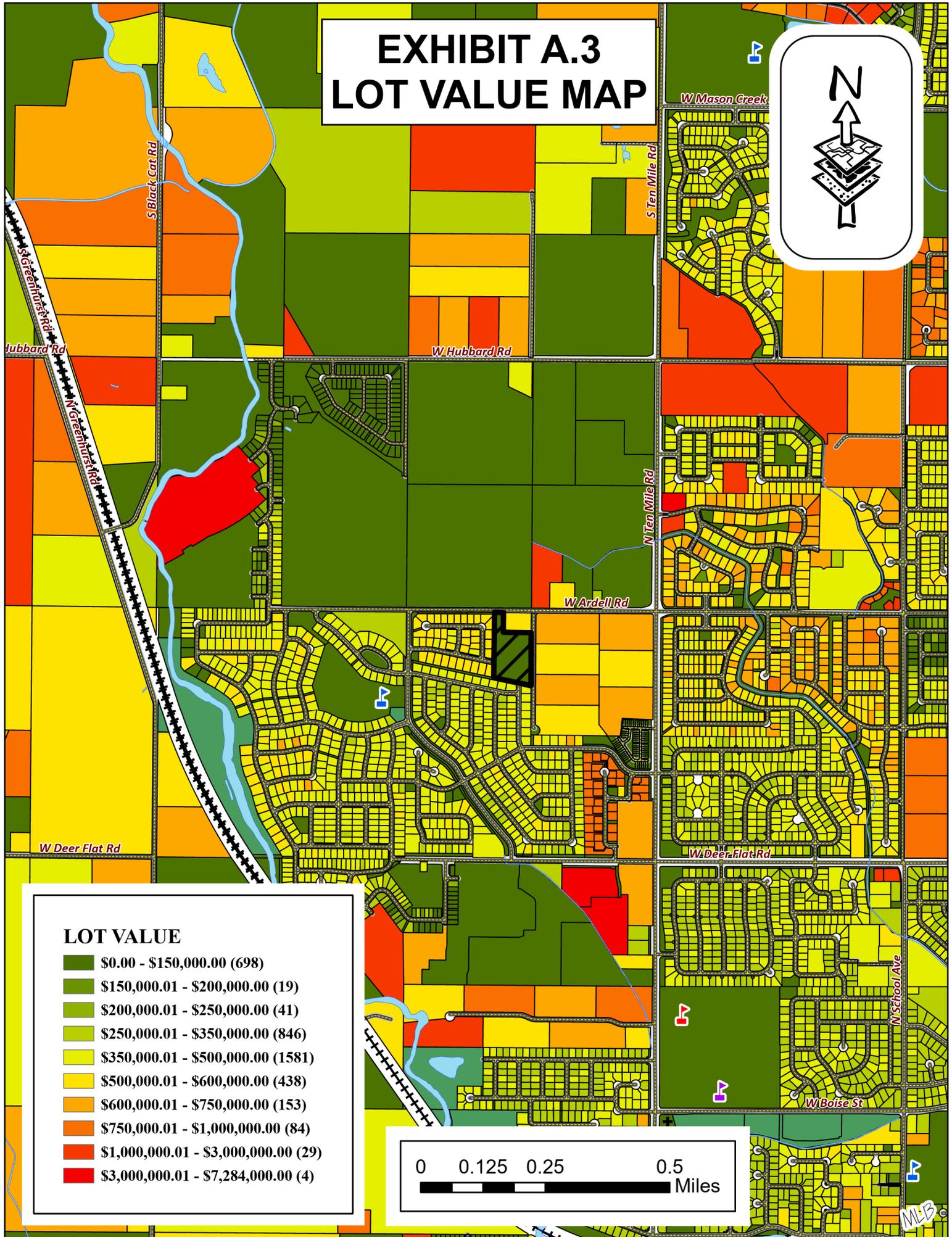
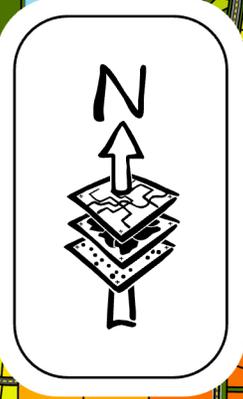
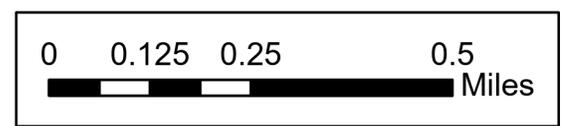


EXHIBIT A.3 LOT VALUE MAP



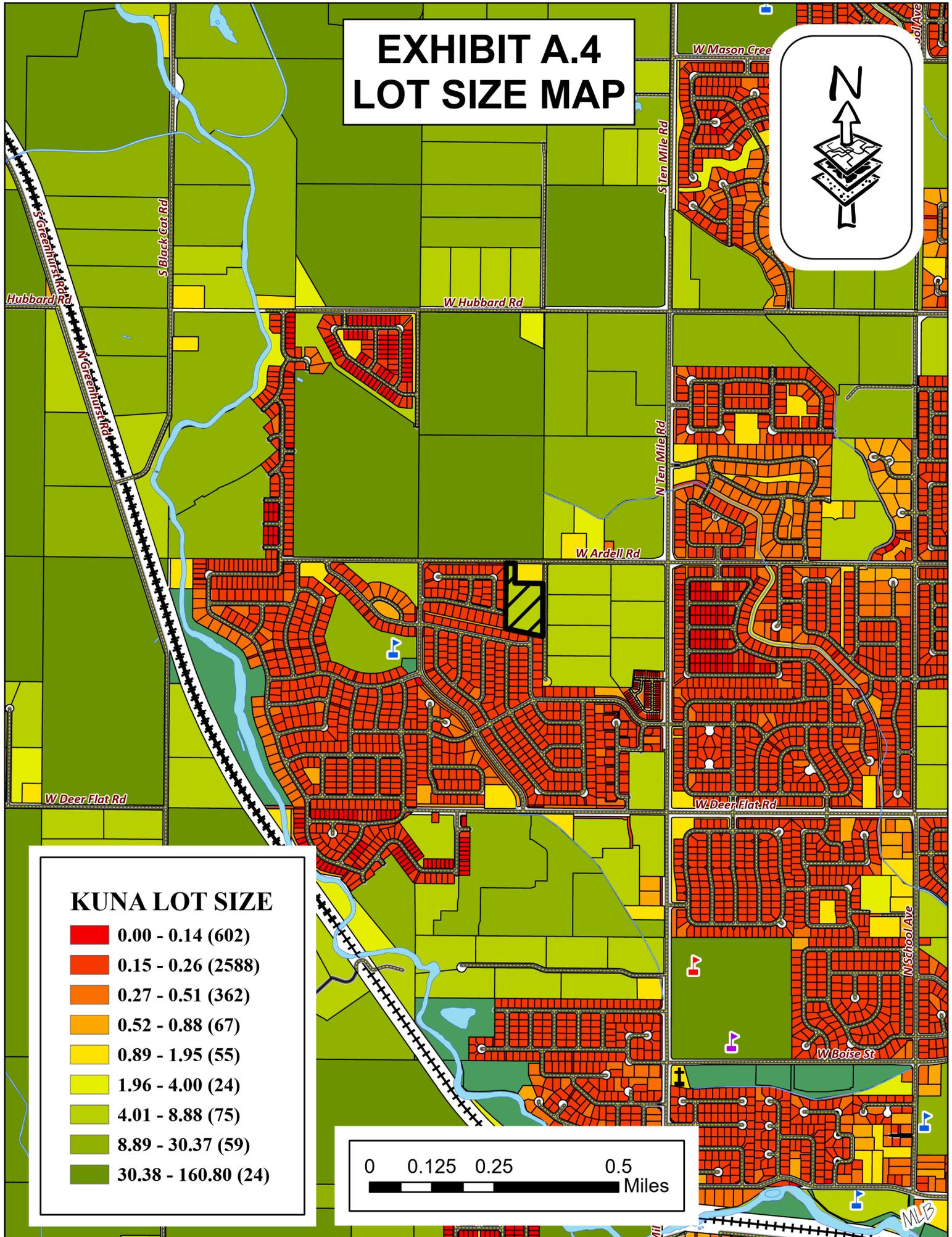
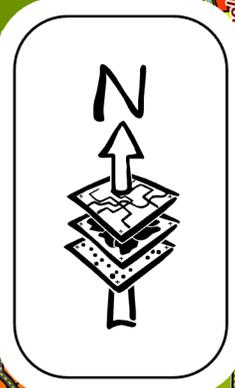
LOT VALUE

	\$0.00 - \$150,000.00 (698)
	\$150,000.01 - \$200,000.00 (19)
	\$200,000.01 - \$250,000.00 (41)
	\$250,000.01 - \$350,000.00 (846)
	\$350,000.01 - \$500,000.00 (1581)
	\$500,000.01 - \$600,000.00 (438)
	\$600,000.01 - \$750,000.00 (153)
	\$750,000.01 - \$1,000,000.00 (84)
	\$1,000,000.01 - \$3,000,000.00 (29)
	\$3,000,000.01 - \$7,284,000.00 (4)



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EXHIBIT A.4 LOT SIZE MAP



KUNA LOT SIZE

- 0.00 - 0.14 (602)
- 0.15 - 0.26 (2588)
- 0.27 - 0.51 (362)
- 0.52 - 0.88 (67)
- 0.89 - 1.95 (55)
- 1.96 - 4.00 (24)
- 4.01 - 8.88 (75)
- 8.89 - 30.37 (59)
- 30.38 - 160.80 (24)

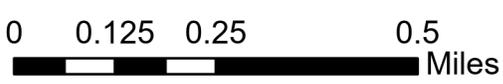
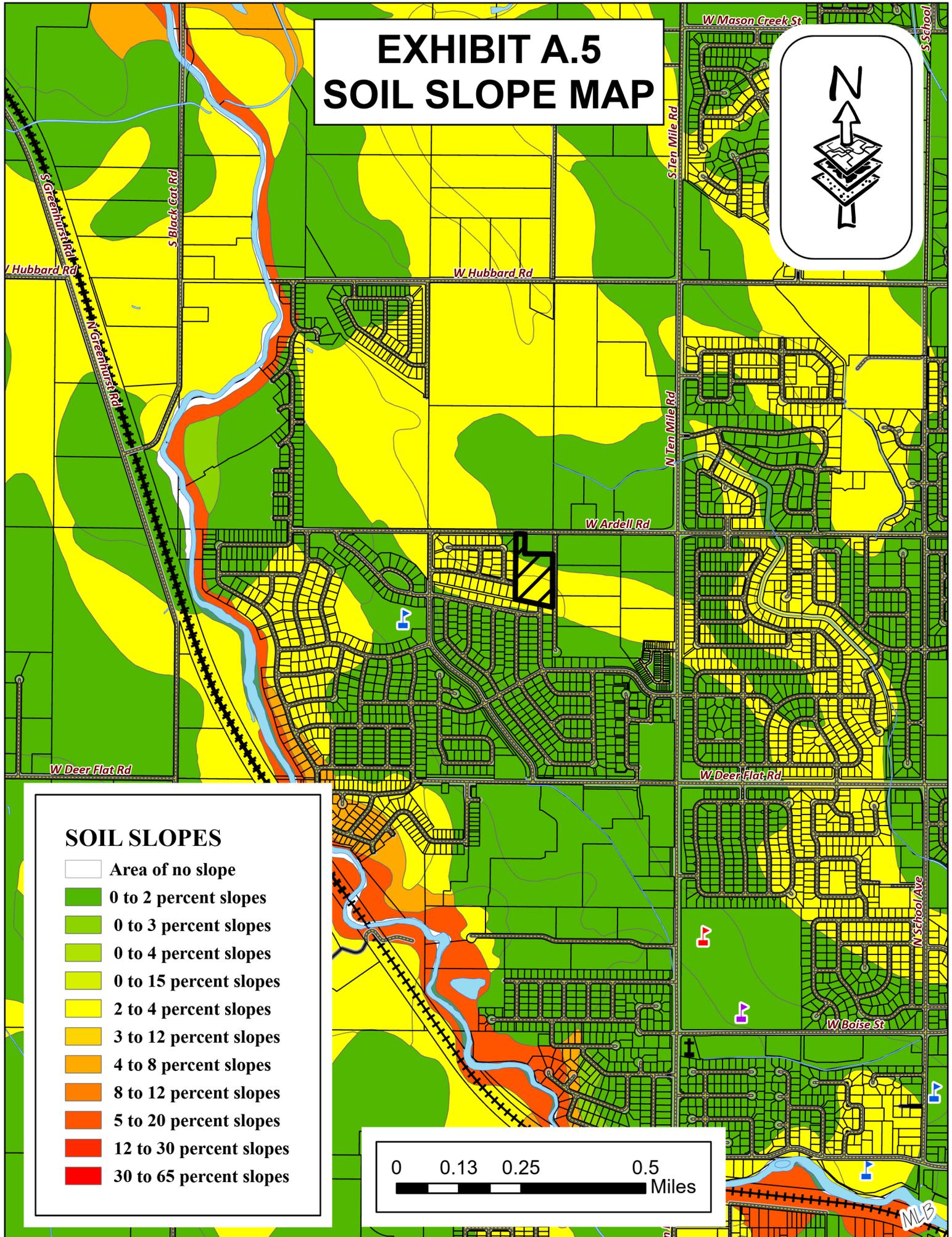
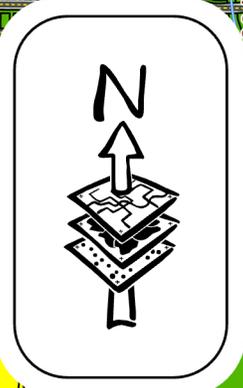
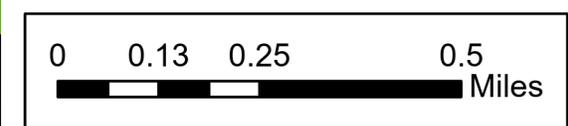


EXHIBIT A.5 SOIL SLOPE MAP



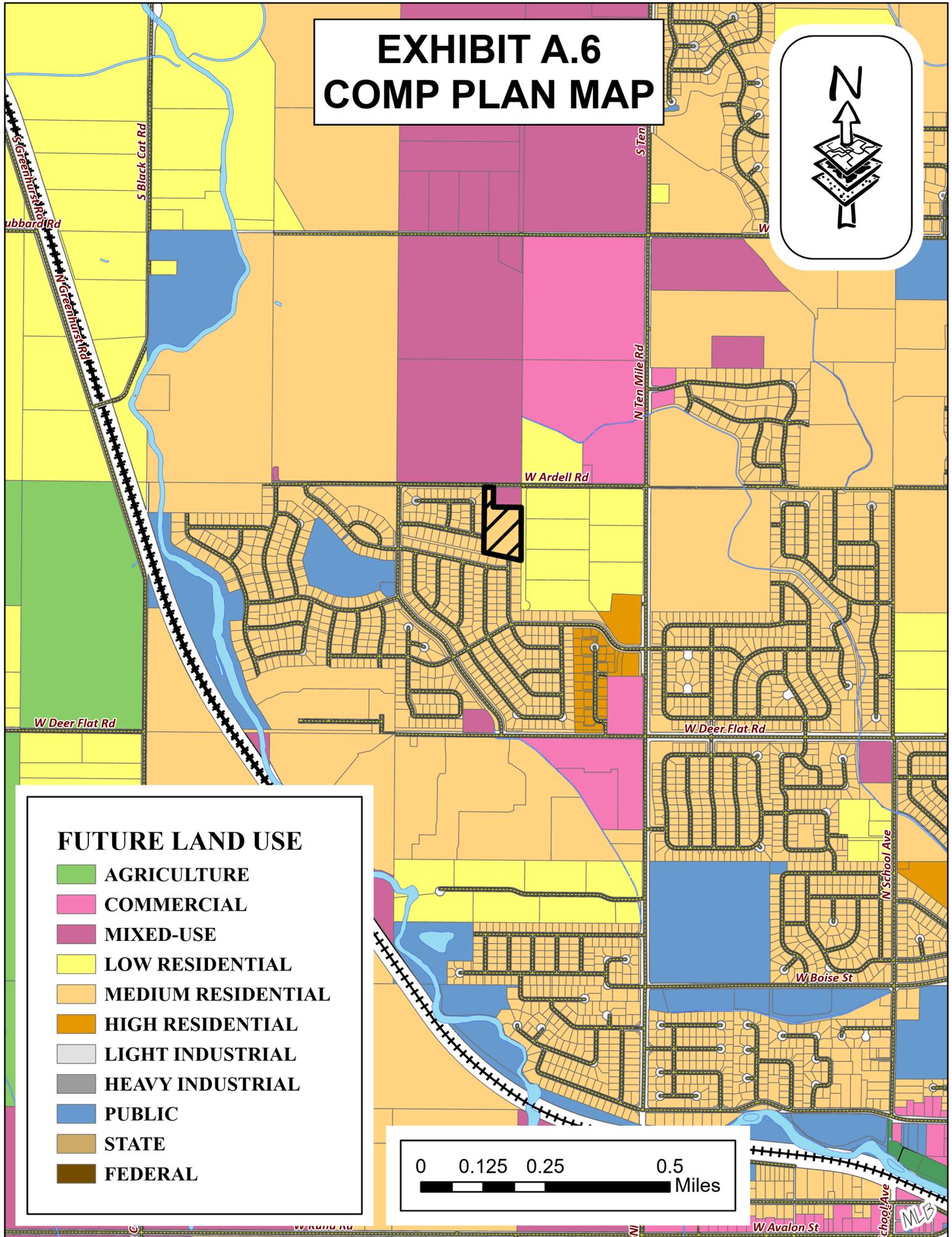
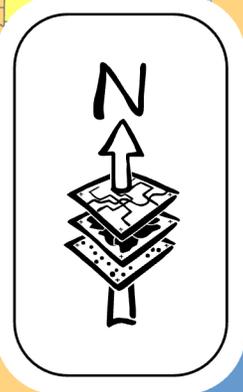
SOIL SLOPES

-  Area of no slope
-  0 to 2 percent slopes
-  0 to 3 percent slopes
-  0 to 4 percent slopes
-  0 to 15 percent slopes
-  2 to 4 percent slopes
-  3 to 12 percent slopes
-  4 to 8 percent slopes
-  8 to 12 percent slopes
-  5 to 20 percent slopes
-  12 to 30 percent slopes
-  30 to 65 percent slopes



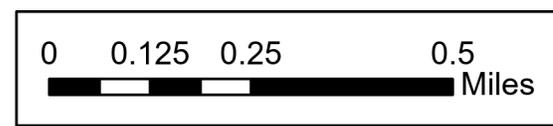
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EXHIBIT A.6 COMP PLAN MAP



FUTURE LAND USE

- AGRICULTURE
- COMMERCIAL
- MIXED-USE
- LOW RESIDENTIAL
- MEDIUM RESIDENTIAL
- HIGH RESIDENTIAL
- LIGHT INDUSTRIAL
- HEAVY INDUSTRIAL
- PUBLIC
- STATE
- FEDERAL



MLB



Kuna School District

Empowering students to lead productive lives.

Date: 11/19/25

RE: Indie Subdivision

Dear Commissioners and City Council Members,

Kuna School District has reviewed the application for Indie Subdivision and provides the following comments for your consideration.

To reduce our reliance on bonds and to promote reasonable growth within our district, we generally seek partnerships with residential developers. Partnerships with developers mitigate the impact homes will have on the Kuna School District and our ability to deliver services without compromising the quality of service delivery to current residents or imposing substantial additional costs upon current residents to accommodate proposed subdivisions or other developments. (*See Idaho Code §67-6513*).

We are currently working with the developer on a partnership with Kuna School District but no official agreement has been reached. An amended letter will be submitted based on the outcome of the continued conversation with the development team.

Regards,

Tim Jensen
KSD Director of Growth & Development

Kim Bekkedahl
Superintendent

Jason Reddy
Asst. Superintendent

Brian Graves
Assistant Superintendent

Tim Jensen
Growth & Development Director

Allison Westfall
Communications Director

Jonathan Gillen
Finance Director

Kelly Schamber
Special Education Director

Kevin Gifford
Curriculum, Instruction &
Assessment Director

DOWNTOWN DESIGN STANDARDS

Planner: Marina Lundy

mlundy@kunaid.gov

986.269.8341

**ALL APPLICATION MATERIALS: [DOWNTOWN
DESIGN STANDARDS](#)**

If you require assistance accessing the application materials through the link provided above or would like to review the application materials in person at City Hall please contact the assigned planner.

October 22, 2025

Planning & Zoning Commission
City of Kuna
751 W 4th Street
Kuna, ID 83634

SUBJECT: Downtown Design Standards

Commissioners,

The City of Kuna Planning and Zoning Department respectfully submits a proposed amendment to the "City of Kuna Downtown Design Standards".

The requested changes are as follows:

1. The removal of the three-color tiers for the Old Town District (Pg. 10) and for the Railroad District (Pg. 15).
2. To replace the existing color palettes on Pg. 10 and Pg. 15 with updated colors.

These changes are being requested to remove outdated colors and replace them with colors that more accurately represent the character of these districts. The removal of color tiers and the addition of one-color palette promotes design flexibility, clarity, and consistency, making the city's design manual easier to use.

Sincerely,

A handwritten signature in black ink, appearing to read "Marina Lundy". The signature is fluid and cursive, with a large, sweeping flourish at the end.

Marina Lundy
Planner

Building Materials

Building exteriors should use traditional materials and colors, with muted historic hues, in keeping with the character of the OTD. The primary building façade should be composed of a combination of stone and brick, or overlapping wood plank siding, with allowable accents of stucco, tile, metal, and/or wood detailing.

Figure 3 shows a typical pair of Kuna Main Street existing buildings and the potential for façade improvements to meet these guidelines.

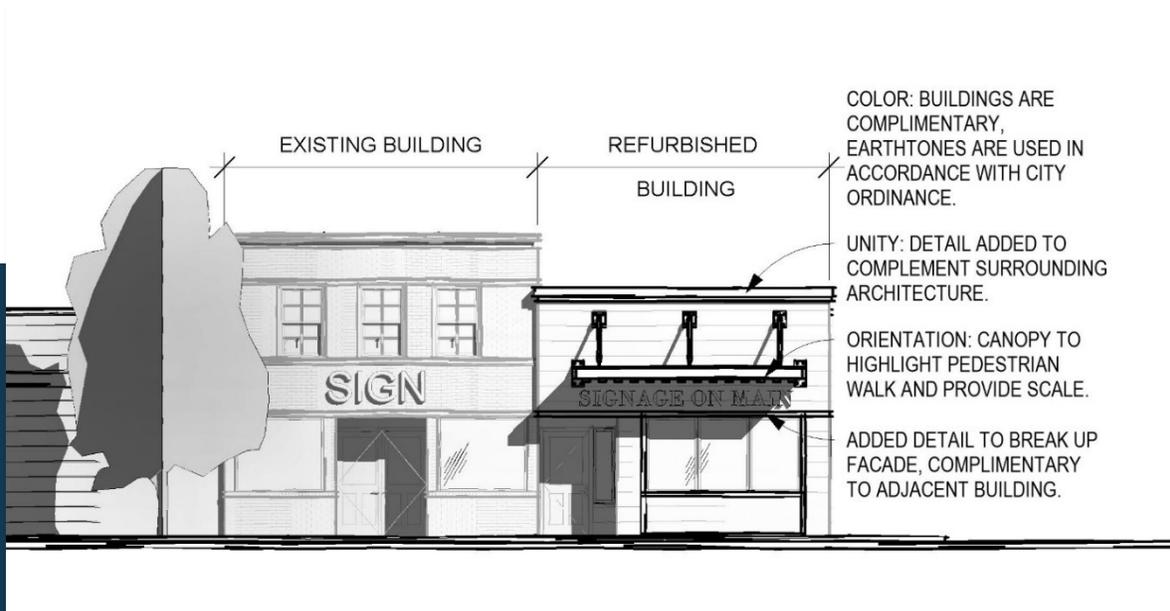
Color

A color palette for the OTD (and NBD) is presented in Figure 4. Flat or low gloss finishes are acceptable, while shiny or high gloss finishes are prohibited. Requested colors outside of the palette shown in Figure 4 shall go before the Design Review Committee for approval.

- Tier 1 colors are intended for the main body of the building, comprising 75 percent or more of the surface area.
- Tier 2 colors are advised for trim on 15 percent or more.
- Tier 3 colors are suggested accents on 10 percent or less of the surface area.



Figure 4: City of Kuna OTD (and NBD) building color palettes



Rendering by Braintree Properties

Figure 3: Example of minor facade treatments to meet design guidelines

BUILDING DESIGN

Roofs

Flat roofs with projecting or decorative cornices and pitched roofs are encouraged to enhance the industrial character of the RRD. Pitched roofs shall have a minimum slope of 4:12 and maximum slope of 12:12. Permanent roof top decks and seating areas are permitted provided they are integrated into the overall building design.

Windows

Each street facing ground level façade should contain between 30 and 70 percent transparent window materials between an average of 2 to 8 feet above grade. Original window styles, grids (arrangement of panes) and materials is required to the furthest extent possible. Mirrored or darkly tinted glass shall not be permitted.

Windows, canopies, and bays on the street façade should be used to provide additional interest at the building entrance.

Building Entrances

Refer to General Standards.

Building Materials

Building exteriors should use materials and colors in keeping with the design and architectural elements found in Southern Idaho railroad building architecture. The primary building façade should be composed of a combination of stone and brick with allowable accents of stucco, tile, metal, and wood detailing.

Color

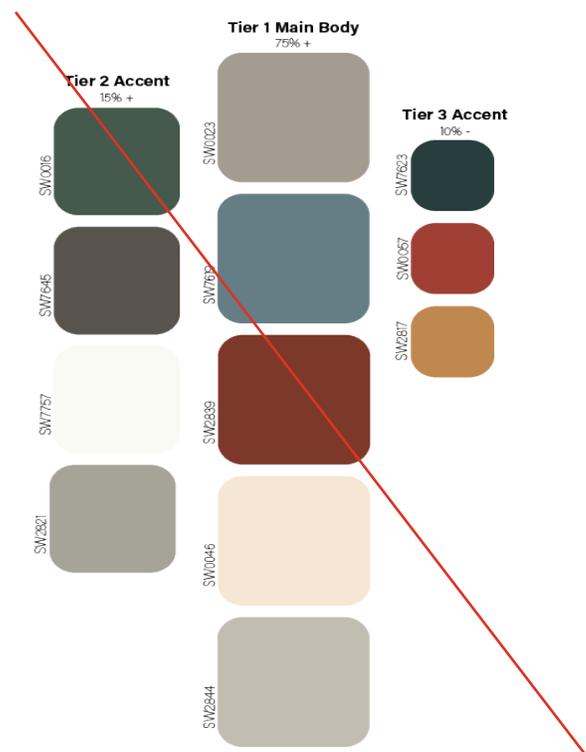
A color palette for the RRD is presented in Figure 12.



Building accents add to the industrial design of Boise Brewing and complementary storefront amenities provide an inviting space for patrons

Flat or low gloss finishes are acceptable, while shiny or high gloss finishes are prohibited. Requested colors outside of the palette shown in Figure 12 shall go before the Design Review Committee for approval.

- ~~☐ Tier 1 colors are intended for the main body of the building, comprising 75 percent or more of the surface area.~~
- ~~☐ Tier 2 colors are advised for trim on 15 percent or more.~~
- ~~☐ Tier 3 colors are suggested accents on 10 percent or less of the surface area.~~



STOREFRONT EXPERIENCE

Figure 12: City of Kuna RRD building color palettes

Signs

Refer to General Standards.

Streetscape

Refer to General Standards.

Building Materials

Building exteriors should use traditional materials and colors, with muted historic hues, in keeping with the character of the OTD. The primary building façade should be composed of a combination of stone and brick, or overlapping wood plank siding, with allowable accents of stucco, tile, metal, and/or wood detailing.

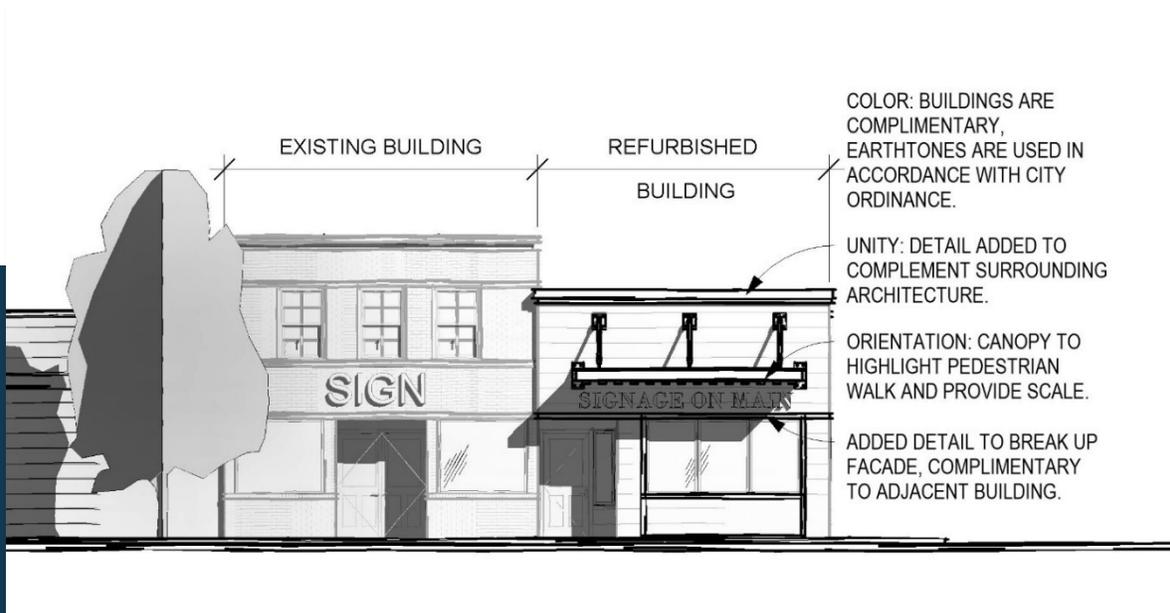
Figure 3 shows a typical pair of Kuna Main Street existing buildings and the potential for façade improvements to meet these guidelines.

Color

A color palette for the OTD (and NBD) is presented in Figure 4. Flat or low gloss finishes are acceptable, while shiny or high gloss finishes are prohibited. Requested colors outside of the palette shown in Figure 4 shall go before the Design Review Committee for approval.



Figure 4: City of Kuna OTD (and NBD) building color palettes



Rendering by Braintree Properties

Figure 3: Example of minor facade treatments to meet design guidelines

BUILDING DESIGN

Roofs

Flat roofs with projecting or decorative cornices and pitched roofs are encouraged to enhance the industrial character of the RRD. Pitched roofs shall have a minimum slope of 4:12 and maximum slope of 12:12. Permanent roof top decks and seating areas are permitted provided they are integrated into the overall building design.

Windows

Each street facing ground level façade should contain between 30 and 70 percent transparent window materials between an average of 2 to 8 feet above grade. Original window styles, grids (arrangement of panes) and materials is required to the furthest extent possible. Mirrored or darkly tinted glass shall not be permitted.

Windows, canopies, and bays on the street façade should be used to provide additional interest at the building entrance.

Building Entrances

Refer to General Standards.

Building Materials

Building exteriors should use materials and colors in keeping with the design and architectural elements found in Southern Idaho railroad building architecture. The primary building façade should be composed of a combination of stone and brick with allowable accents of stucco, tile, metal, and wood detailing.

Color

A color palette for the RRD is presented in Figure 12.



Flat or low gloss finishes are acceptable, while shiny or high gloss finishes are prohibited. Requested colors outside of the palette shown in Figure 12 shall go before the Design Review Committee for approval.



Figure 12: City of Kuna RRD building color palettes

STOREFRONT EXPERIENCE

Signs

Refer to General Standards.

Streetscape

Refer to General Standards.

Building accents add to the industrial design of Boise Brewing and complementary storefront amenities provide an inviting space for patrons



CITY OF KUNA

DOWNTOWN DESIGN STANDARDS

July 2, 2024
Amendments Page 53

**RESOLUTION NO. R40-2024
CITY OF KUNA, IDAHO**

A RESOLUTION OF THE CITY COUNCIL OF KUNA, IDAHO:

- **MAKING CERTAIN FINDINGS AND DECLARATION OF AUTHORITY; AND**
- **ADOPTING THE “CITY OF KUNA DOWNTOWN DESIGN STANDARDS”; AND**
- **DIRECTING THE CITY CLERK; AND**
- **PROVIDING AN EFFECTIVE DATE.**

NOW, THEREFORE BE IT RESOLVED by the Mayor and City Council of the City of Kuna, Ada County, state of Idaho (the “City”):

Section No. 1. Findings:

- 1.1 The City is a municipal corporation established in accordance with Article XII of the Constitution of the State of Idaho and Title 50 Idaho Code; and
- 1.2 On November 19, 2019, the City entered into a Professional Services Agreement with JUB Engineering, Inc. for developing downtown design standards as attached hereto as EXHIBIT B.
- 1.3 JUB Engineering, Inc. completed the downtown design standards in July 2021.
- 1.4 The downtown design standards accompanied Case No. 23-01-OA (Ordinance Amendment) Kuna City Code Title 5 & 6 Rewrite through the public hearing process.
- 1.5 It being the City Council’s intent and purpose in approving this Resolution to adopt the “City of Kuna Downtown Design Standards” upon the effective date of this Resolution.

Section 2: Action:

- 2.1 The City Council hereby adopts, approves, and enacts the “City of Kuna Downtown Design Standards” as attached hereto as EXHIBIT A.

Section 3: Directing the City Clerk

- 3.1 The City Clerk is directed to file this Resolution forthwith in the official records of the City.

Section 4. Effective Date

- 4.1 This Resolution shall take effect and be in force from and after its passage and approval.

PASSED by the Council for the City of Kuna, Idaho this 2nd day of July, 2024.

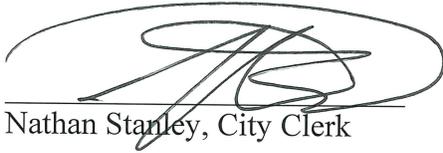
APPROVED by the Council for the City of Kuna, Idaho this 2nd day of July, 2024.

CITY OF KUNA



Joe L. Stear, Mayor

ATTEST:



Nathan Stanley, City Clerk



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Appendices

Appendix A Streetscape Specifications

Definitions

The words and terms below have been used throughout this document and are defined here as follows:

Alcove | a small building recessed space that may open into a larger space

Awning | a canvas or other temporary material used to create a covering over a window or door

Balcony | a platform projection from the wall or window, enclosed by a railing

Balustrade | a series of balusters (short pillars with a top and bottom rail) used on staircases, balconies, and porches

Band | any horizontal member that projects slightly from the surface, often used to mark a division on a wall (also known as band course or band molding)

Bay | a structural wall projection of three sides from the ground up often including windows

Bracket | a projection from a vertical surface, often providing support (balcony, roof, etc.) or as a fastener

Canopy | a projection over a niche or doorway

Cornice | a horizontal molded projection typically provided as a decorative element

Court | an open area totally or partially surrounded by walls or buildings

Deck | a flat surface capable of supporting weight

Façade | the principal face or front elevation of a building

Forecourt | a court situated at the building front or entryway

Form | the overall shape or outline of a building

Gable | a pitched roof in the shape of a triangle

Hipped | a roof with slopes on all sides

Hue | a shade or tint of a color

Infill building | a new structure built in a block of existing buildings

Masonry | work constructed by a mason using stone, brick, tile, or other similar materials

Mass | the overall size or bulk of a building

Pane | a single piece of window glass

Panel | a sunken or raised portion of a wall, ceiling, or door with a frame-like border

Parapet | a low wall that rises above a roofline or porch

Pedestrian-friendly | provides design and amenities that encourage residents and visitors to walk and enjoy their surroundings

Pitch | the degree of slope of a roof

Porch | a covered entrance or semi-enclosed space projecting from the building façade

Scale | the relationship of a building to its surroundings (site, neighboring properties, etc.)

Style | design characteristics and elements that form a type or group associated with a design philosophy or time-period

Tile | a piece of fired clay that is thinner than a brick

Transparency | the amount of visibility within a building plane provided by windows and other openings

Trim | edge or finish around a door or window

Visual continuity (or Unity) | an appearance of belonging together presented in structures that share common style elements or other physical similarities

1 | Introduction

In 2015, the City of Kuna adopted a Downtown Revitalization Plan, followed by an updated Comprehensive Plan in 2019, *Envision Kuna*, that recommended several downtown revitalization strategies, including the development of Downtown Design Standards (DDS). The DDS offer intentional guidelines beyond the City Code and policies to implement the community’s vision, encourage design identity, and provide visual cohesion.

DOWNTOWN DISTRICTS

The Downtown Design Standards apply to three subarea districts, along with Entrance Corridors, as described below and shown in Figure 1.

ENTRANCE CORRIDORS

Streetscape elements along four corridors to welcome residents and visitors as they approach the downtown core. More information is provided in the General Standards section.

REVIEW PROCESS

At a minimum, Administrative Design Review will be required for any exterior modification, rehabilitation, reconstruction, new structures, etc. within all Downtown Districts and Entrance Corridors.



Old Town District (OTD)

Kuna’s historic townsite, including Main Street, commercial and civic uses in older structures, some dating from Kuna’s early days as a railroad settlement.



Neighborhood Business District (NBD)

Adjacent to the OTD, predominantly residential and some commercial uses, including properties along 4th Street, Owyhee and Park Avenues, and Avalon Addition.



Railroad District (RRD)

South of the OTD along the rail line, including the Indian Creek Greenbelt and the proposed Helen Zamzow Park.

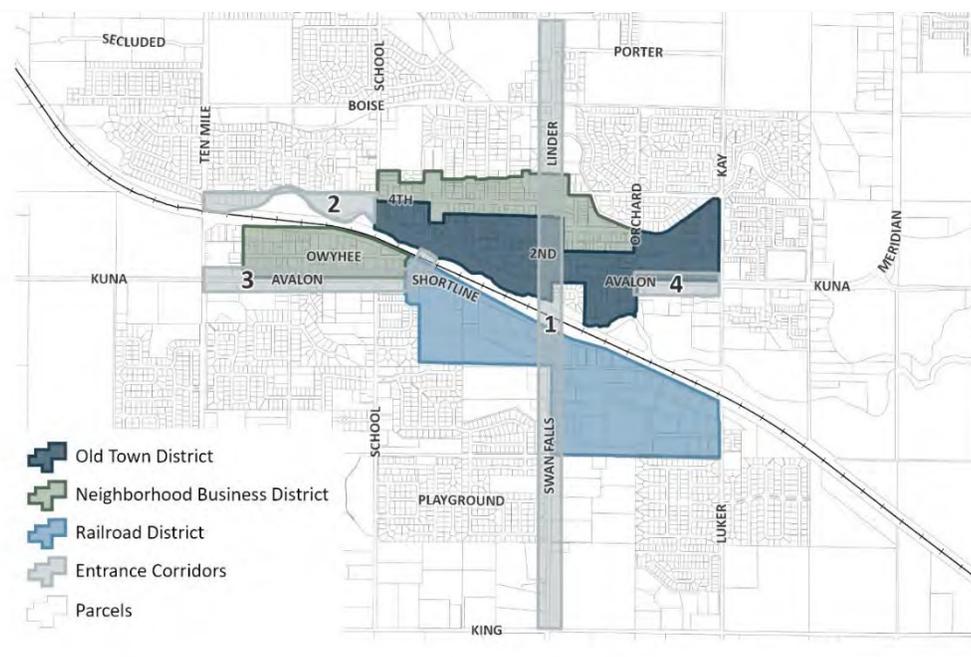


Figure 1:
Kuna Downtown Subarea Districts

2 | General Standards

The following General Standards are applicable to all three subarea Districts and Entrance Corridors.

SITE LAYOUT & ORIENTATION

Parking

Off-street parking, including surface parking lots, shall be located to the rear or sides of buildings. When surface parking lots abut a public street, they must be screened with a treatment, at least 2 feet in height and no more than 3 feet in height, consisting of landscape plantings of evergreen shrubs and groundcover materials or low walls made of concrete, masonry, or other similar material, or raised planters planted with evergreen shrubs. If walls or raised planters must exceed 3 feet in height, it must be demonstrated that the screening treatment does not create a safety hazard, and the portion of treatment that is above 3 feet in height is a minimum 75% transparent (i.e. railing, trellis, or other similar treatment) and provides added visual interest and detail enhancing the District character.

If there is insufficient space to allow the use of the above screening treatments, a decorative railing or fencing may be considered. Chain link fencing is prohibited.

Lighting

On premise, outdoor lighting must meet Kuna City Code requirements, including dark sky applications. Businesses are encouraged to rely on ambient street lighting for the safe illumination of the sidewalk area. Low wattage lighting of entryways, window spaces and indoor spaces should occur in such a manner that no glare is cast on to the sidewalk area. Flashing and strobe lighting is prohibited. See appendices for public right-of-way and other public property lighting specifications.

BUILDING DESIGN

Building Entrances

Building entrances should be easily identifiable and accessible. Features such as flanking windows or canopies may be used, as well as recessed entries,

balconies, and forecourts. The street level should offer interest through the placement of arches, bays, and canopies. Further decoration and articulation with projecting sills, bands, or other features, using acceptable building materials, is also encouraged.

STOREFRONT EXPERIENCE

Storefront/Outdoor Seating or Shopping

Areas for customers to enjoy shopping, eating, or drinking outdoors should be integrated into setback areas, side yards and surface parking areas. In some districts, where sufficient front setbacks do not occur, outdoor seating may occur within the right-of-way. Sidewalk seating, shopping areas or storefront amenities may be permitted provided that a minimum of 5-foot-wide continuous pedestrian access along the sidewalk is provided, without obstructions or hazards to pedestrian activity.

Further provisions regarding service of alcohol, fencing, and lighting may also be enforced. Materials and color of semi-permanent fixtures shall complement the area and blend with the building in compliance with these standards.

Any proposed seating, shopping or alternative activities within alleyways, roadways, or public right-of-way must be approved by the City and Ada County Highway District (ACHD).



Figure 2: Rendering of a downtown Nampa alleyway

Signs

Business signage shall be architecturally compatible and in scale with the building, fitting within the features of the building façade, such as above windows, within entry ways, above or beside windows. Colors and materials should complement the overall appearance of the building. Business signage design criteria:

- ❑ Shallow wall-mounted, either against the façade or perpendicular (blade signs) and may be framed or painted directly on the wall. Blade signs supported by ornamental bracket and oriented to the pedestrian are encouraged.
- ❑ Painted wood or enameled metal signs are preferred.
- ❑ Low, free-standing signs may be considered for sites with adjacent open areas, such as surface parking areas or outdoor seating areas.
- ❑ Refrain from concealing significant architectural features or details and/or projecting above the roof, parapet, or exterior wall.
- ❑ Plastic and internally lit signs are strongly discouraged.
- ❑ Electronic message centers are not permitted.

ENTRANCE CORRIDORS

The purpose of the Entrance Corridors is to welcome residents and visitors as they approach the downtown core from all directions. Public streetscape improvements within Entrance Corridors and all Downtown Subarea Districts should mirror Main Street to the furthest extent possible. Streetscape improvements may include wider sidewalks, decorative lighting, banners, colored and stamped concrete, intersection bulb-outs, pedestrian crossings, street trees and grates, bike racks, art, and trash receptacles. Any new or modified streetscape improvements must be approved by the City of Kuna and ACHD. Streetscape specifications (concrete color, stamp patterns, street lighting, etc.) are provided in Appendix A.



Outdoor seating along the sidewalk in front of the Sun Ray Cafe in Boise's Hyde Park



Various complementary signs at a Hyde Park bike shop



Big Picture

Retain historic character with redevelopment of older buildings, provide thoughtful use of in-fill that respects adjacent structures and optimizes use, and maintain character defining elements during redevelopment and rehabilitation of older buildings, to keep Kuna unique.

3 | Old Town District

SITE LAYOUT & ORIENTATION

Building Orientation

Buildings should be oriented to/facing the street, with special attention to the rhythm and proportion of building entrances and windows to provide visual interest for the pedestrian.

Buildings should be at the street property line, with parking provided to the rear or off site, with appropriate use of landscaping to complement the structure and provide screening as needed, thus creating a pedestrian-friendly edge.

Setbacks

Whenever possible, buildings should be located adjacent to the sidewalk, in keeping with the placement of older buildings on Main Street. If usable public space, such as outdoor dining, or court amenities are provided, a setback of no more than 20 feet may be considered. In areas outside of Main Street within the OTD, zero setbacks are highly encouraged to extend the character of the historic townsite and promote pedestrian activity.

Height

The maximum building height is determined by the underlying zone. In keeping with the historic character, buildings should not exceed 4 stories. Buildings more than 2 stories must be stepped back from upper stories to reduce bulk and provide architectural interest.

BUILDING DESIGN

Roofs

Flat roofs with projecting or decorative cornices are encouraged to preserve the existing character of the OTD. Permanent roof top decks and seating areas are



Teed family photo collection

The unique historic character of Main Street is still reflected in downtown Kuna today

permitted provided they are integrated into the overall building design.

Windows

Windows, alcoves, and bays on the street façade should be used to provide additional interest at the building entrance.

Each street facing ground level façade should contain between 30 and 70 percent transparent window materials and occur between an average of 2 to 8 feet above grade. Original window styles, grids (arrangement of panes) and materials is required to the furthest extent possible. Mirrored or darkly tinted glass is not allowable.

Building Entrances

Building entrances should be easily identifiable and accessible. Features such as flanking windows or canopies may be used, as well as recessed entries, balconies, and forecourts. The street level should offer interest through the placement of arches, bays, and canopies. Further decoration and articulation with projecting sills, bands, or other features, using acceptable building materials, is also encouraged.

Building Materials

Building exteriors should use traditional materials and colors, with muted historic hues, in keeping with the character of the OTD. The primary building façade should be composed of a combination of stone and brick, or overlapping wood plank siding, with allowable accents of stucco, tile, metal, and/or wood detailing.

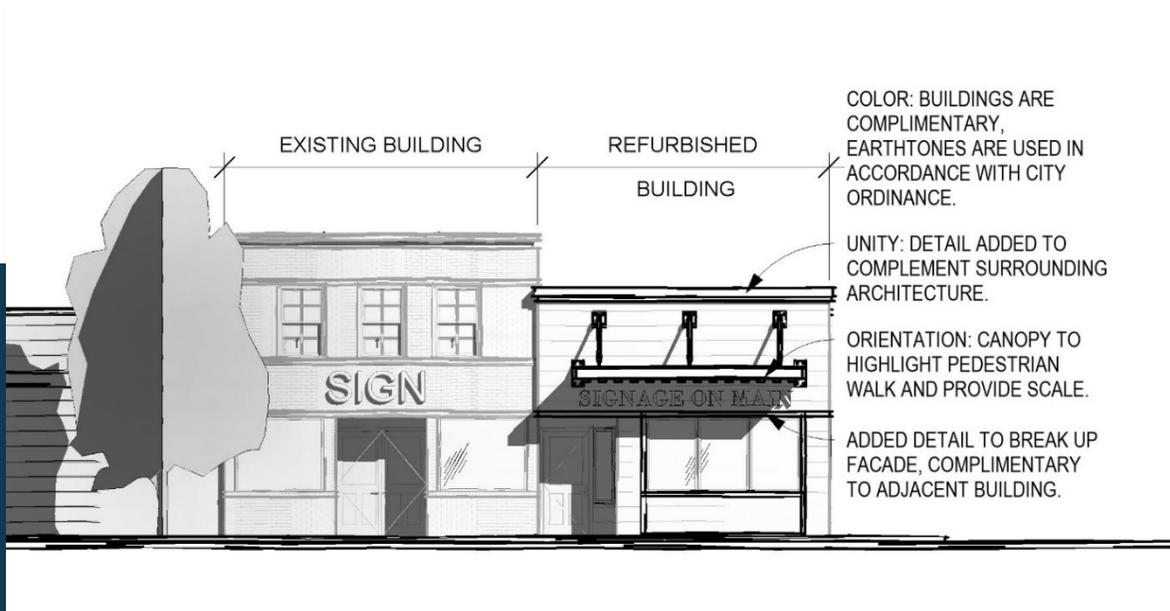
Figure 3 shows a typical pair of Kuna Main Street existing buildings and the potential for façade improvements to meet these guidelines.

Color

A color palette for the OTD (and NBD) is presented in Figure 4. Flat or low gloss finishes are acceptable, while shiny or high gloss finishes are prohibited. Requested colors outside of the palette shown in Figure 4 shall go before the Design Review Committee for approval.



Figure 4: City of Kuna OTD (and NBD) building color palettes



Rendering by Braintree Properties

Figure 3: Example of minor facade treatments to meet design guidelines

Figure 5 depicts typical Main Street buildings with the recommended materials and color palette applied.

STOREFRONT EXPERIENCE

Signs

In addition to the guidelines in the General Standards section, business signage should be shallow wall-mounted, either against the façade or perpendicular (blade signs) and may be framed or painted directly on the wall.

Streetscape

The typical Section for Main Street is shown below in Figure 6. Streetscape improvements should mirror Main Street to the furthest extent possible, incorporating the specifications provided in Appendix A. For any deviation from this Typical Section within the OTD, appropriate planning processes and approvals by ACHD and the City of Kuna shall occur.



Rendering by Braintree Properties

Figure 5: Rendered example of facade upgrades complementary and unifying to existing buildings



Figure 6: Kuna Main Street Typical Section – larger version can be found in Appendix A



Big Picture

Provide a unique environment that retains a mix of residential and commercial uses, maintaining the existing historic setting by repurposing with improved facilities, and enhance the downtown experience with outdoor activity.

4 | Neighborhood Business District

SITE LAYOUT & ORIENTATION

Building Orientation

Buildings should be oriented to/facing the street or toward side yards, with special attention to the areas between buildings to provide pedestrian amenities and points of visual interest.

Buildings should be situated behind a front yard, with parking provided to the rear or off site, and with appropriate use of landscaping or outdoor seating to complement the structure and provide screening as needed.

Setbacks

Whenever possible, buildings should be located no less than 20 feet from the street in front and 15 feet from any street along the side. Setback areas should be landscaped, particularly along the right of way, and should be used to accommodate outdoor dining, merchandise display or to provide other court amenities. Figure 7 shows how the setback area can be purposed for outdoor seating.

Height

The maximum building height is determined by the underlying zone. In keeping with the NBD's residential character, a maximum of 2 stories is recommended.



A bungalow in the Neighborhood Business District reflects unique character



Goody's ice cream shop in Boise's Hyde Park provides a landscaped edge to their front and side yard outdoor seating area

Figure 7: Example of residential use adapted for commercial use, including outdoor seating in the front yard area



Rendering by Braintree Properties

BUILDING DESIGN

Roofs

Pitched roofs with a minimum slope of 4:12 and maximum slope of 12:12 are encouraged to preserve the existing character of the NBD. It is also encouraged to preserve historical elements distinct to the district, including gabled and hipped roof shapes and detailing such as large eaves, exposed beam, and brackets.

Windows

Each street facing ground level façade should contain between 15 and 60 percent transparent window materials between an average of 2 to 8 feet above grade. Original window styles, grids (arrangement of panes) and materials is required to the furthest extent possible. Mirrored or darkly tinted glass is not allowable.

Windows, bays, and porches on the street façade should be used to provide additional interest at the building entrance.

Building Entrances

Refer to General Standards.

Building Materials

Building exteriors should use the same suitable building materials as the OTD. Refer to Page 5.

Color

Building exteriors should use the same color palette as the OTD. Refer to Figure 4.



A business in Ketchum, Idaho with decorative fencing and a complementary sign that enhances the overall street facade

STOREFRONT EXPERIENCE

Signs

In addition to the guidelines outlined in the General Standards section, business signage should be shallow wall-mounted, either against the façade or perpendicular (blade signs). Hanging signs may be permitted on structures. Low, free-standing signs may be integrated with fences and walls surrounding yard areas, accessory buildings, and surface parking areas.

Streetscape

Streetscape improvements should incorporate the specifications provided in Appendix A. For any deviations from these Typical Sections in the NBD, appropriate planning processes and approvals by ACHD and the City of Kuna shall occur.

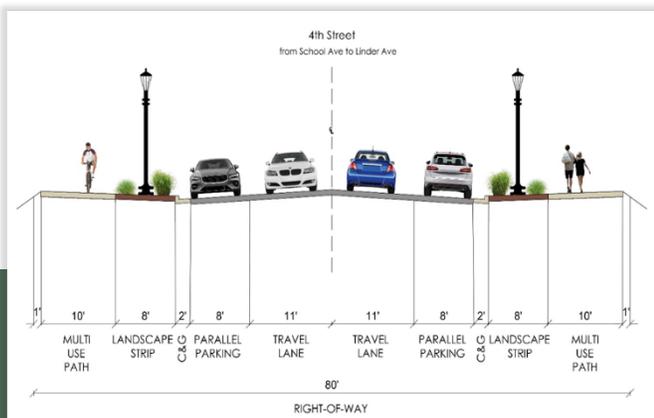


Figure 8: Typical Section for 4th Street – larger version can be found in Appendix A



Big Picture

Reference Kuna's railroad history with design of new buildings and amenities, increase recreation and downtown connectivity through Indian Creek Greenbelt connections and improvements, and expand dining and entertainment opportunities.

5 | Railroad District

SITE LAYOUT & ORIENTATION

Building Orientation

Buildings should be oriented to/facing the street or toward side yards, with special attention to the areas between buildings to provide courts and other areas of interest.

Buildings should be situated behind the sidewalk whenever possible, with parking provided to the rear, and with appropriate use of landscaping or architectural treatments to complement the structure and provide screening as needed.

Where applicable, buildings may be oriented toward Indian Creek to create visual interest and expand pedestrian activity and recreation access. See Figure 10 and Figure 11.

Setbacks

Whenever possible, buildings should be located adjacent to the sidewalk. If usable public space, such as outdoor dining, or court amenities are provided, a setback of no more than 20 feet may be considered.



Figure 10: Rendered waterfront building (Kuna Comprehensive Plan, 2015)



While Kuna's historic depot was demolished, the impact of the Oregon Short Line Railroad to Kuna's development has not been forgotten

Height

The maximum building height is determined by the underlying zone. Buildings more than 3 stories in height must be stepped back from upper stories to reduce bulk and provide architectural interest.

Parking

In addition to the guidelines outlined in the General Standards section, parking structures may be permitted to the rear or sides of buildings or above levels with other uses. Ground level views into a parking structure must be screened with a treatment, consisting of evergreen shrubs or vines, or decorative metal panels or opaque glass.



Figure 11: Rendered recreation/entertainment corridor (Kuna Comprehensive Plan, 2015)

BUILDING DESIGN

Roofs

Flat roofs with projecting or decorative cornices and pitched roofs are encouraged to enhance the industrial character of the RRD. Pitched roofs shall have a minimum slope of 4:12 and maximum slope of 12:12. Permanent roof top decks and seating areas are permitted provided they are integrated into the overall building design.

Windows

Each street facing ground level façade should contain between 30 and 70 percent transparent window materials between an average of 2 to 8 feet above grade. Original window styles, grids (arrangement of panes) and materials is required to the furthest extent possible. Mirrored or darkly tinted glass shall not be permitted.

Windows, canopies, and bays on the street façade should be used to provide additional interest at the building entrance.

Building Entrances

Refer to General Standards.

Building Materials

Building exteriors should use materials and colors in keeping with the design and architectural elements found in Southern Idaho railroad building architecture. The primary building façade should be composed of a combination of stone and brick with allowable accents of stucco, tile, metal, and wood detailing.

Color

A color palette for the RRD is presented in Figure 12.



Flat or low gloss finishes are acceptable, while shiny or high gloss finishes are prohibited. Requested colors outside of the palette shown in Figure 12 shall go before the Design Review Committee for approval.



Figure 12: City of Kuna RRD building color palettes

STOREFRONT EXPERIENCE

Signs

Refer to General Standards.

Streetscape

Refer to General Standards.

Building accents add to the industrial design of Boise Brewing and complementary storefront amenities provide an inviting space for patrons



Big Picture

Ensure that new construction meets the design standards described for the appropriate downtown district to retain and enhance Kuna’s unique character.

6 | New Construction

The guidance provided for each subarea should address most issues related to new construction, particularly infill development. Some additional guidance regarding site planning and placement is provided in this chapter. The most impactful change anticipated from these standards are building placement within the site, which will be more oriented to the sidewalk and street, with parking located internal to the site, as opposed to parking being located along the sidewalk and street.

Figure 13 is an example site plan for new development within the Railroad District or Entrance Corridor no. 4 (Kuna/Meridian Road). Illustrated are key elements of the site and an approach to utilizing a large lot with multi-story buildings and integrated surface parking lots.

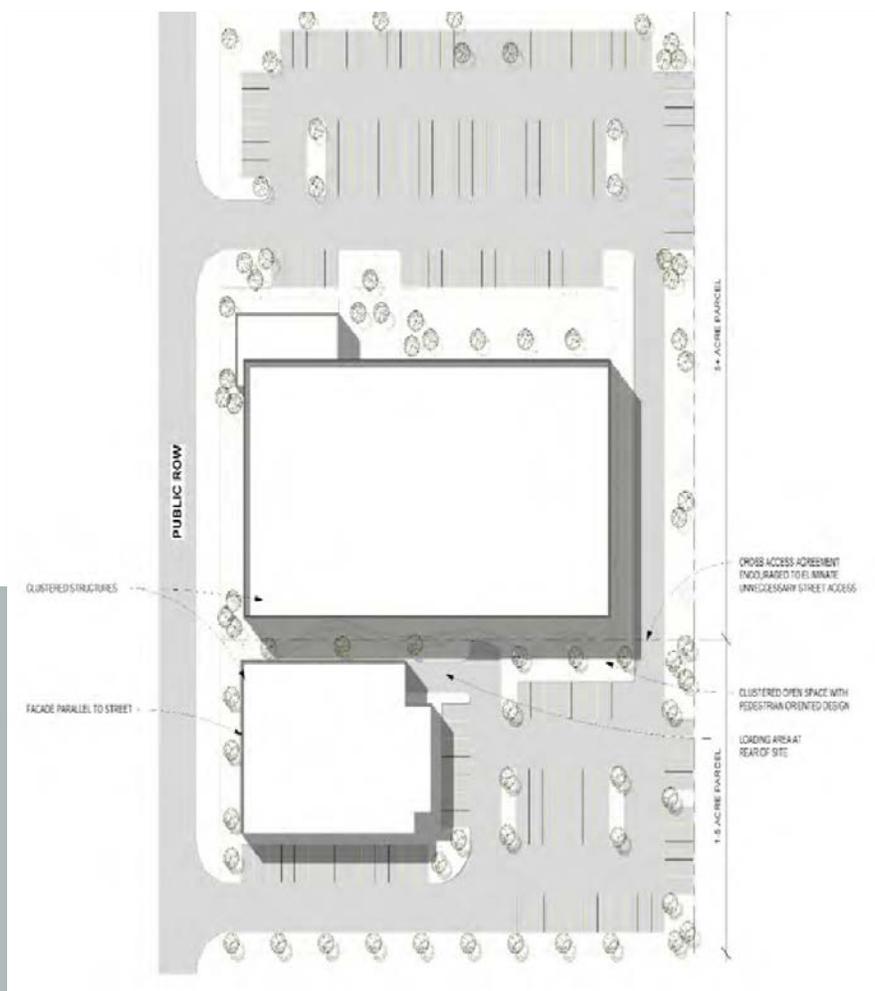


Figure 13: Site plan depicting potential new development within the RRD or Entrance Corridor no. 4

Rendering by Braintree Properties

Site planning and placement of new structures should consider the existing context of the relevant downtown subarea, the location of surrounding land uses and transportation modes, and a site's unique characteristics. Development applications for new construction should heed the provisions for their relevant subarea regarding mass and form, orientation and openings, and materials and colors. Kuna City Code provides specifications regarding parking, lighting, outdoor storage, and landscaping.

Other fundamentals of new construction include:

- ❑ Structures should be designed and sited in a manner that complements any adjacent structures. Sites should be developed in a coordinated manner, that allows diversity of use yet functions cohesively.
- ❑ If possible, new structures should be clustered to create plazas or pedestrian malls and prevent long rows of structures. When clustering is impractical, a visual link between separate structures should be established using visual cues such as similar design elements or landscape treatments.
- ❑ Locate structures and on-site circulation systems to minimize pedestrian vehicle

conflicts. Link structures to the public sidewalk with textured paving and other landscape treatments.

- ❑ Ensure spaces between structures have adequate area and clear edges that reflect careful planning; these spaces should provide pedestrian amenities such as shade and benches, unique lighting, or plantings.
- ❑ Freestanding commercial structures should be oriented with their major entry toward the street, where access is provided, and have their major façade parallel to that street.
- ❑ Loading facilities should not be located at the front of structures, rather they should be located at the rear of the site.

Open spaces should be clustered into larger landscaped areas, rather than in shallow strips, so they may provide more impact as buffers or usable space for pedestrians.



Figure 14: Rendered example of plaza-style development with parking and a mix of commercial uses (Kuna Comprehensive Plan, 2015)

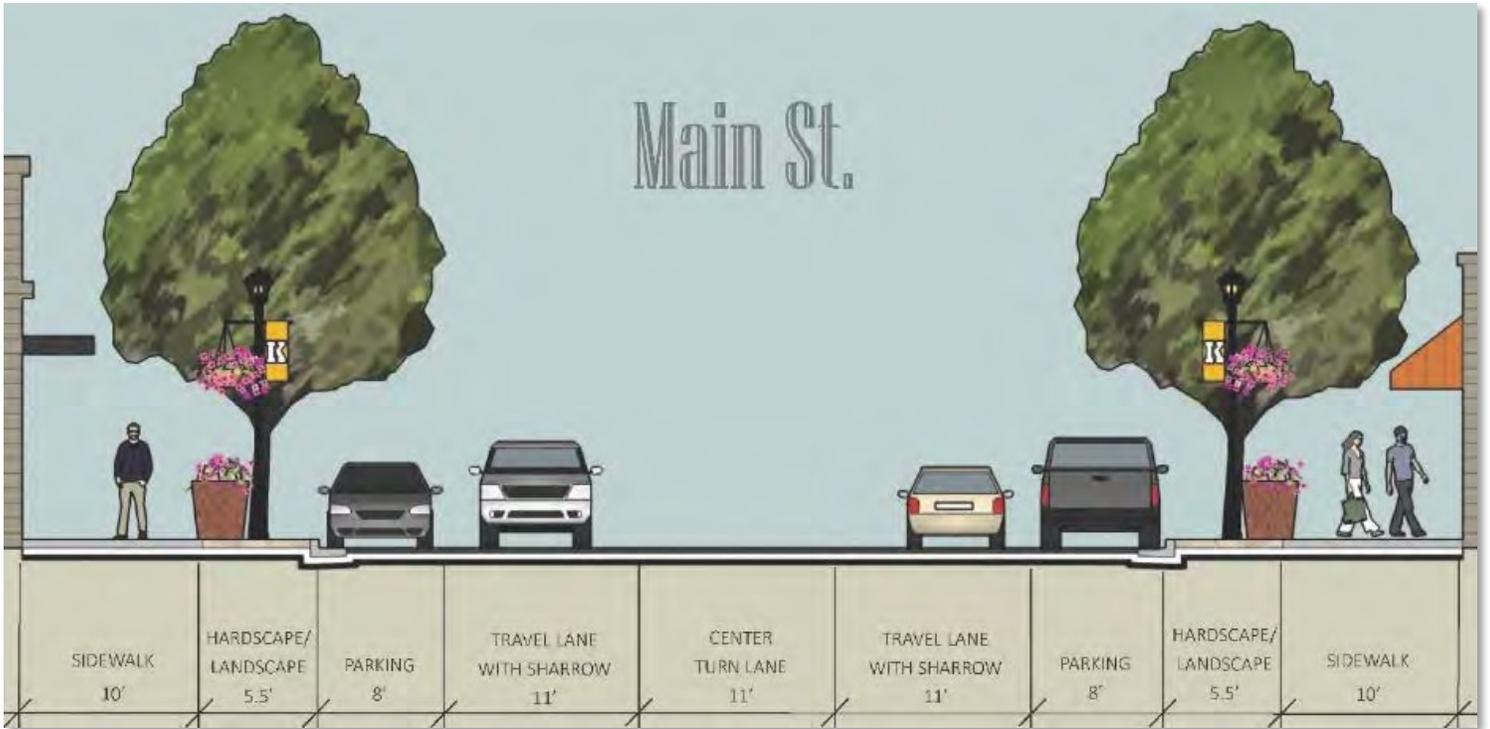


Appendix A

Streetscape Specifications

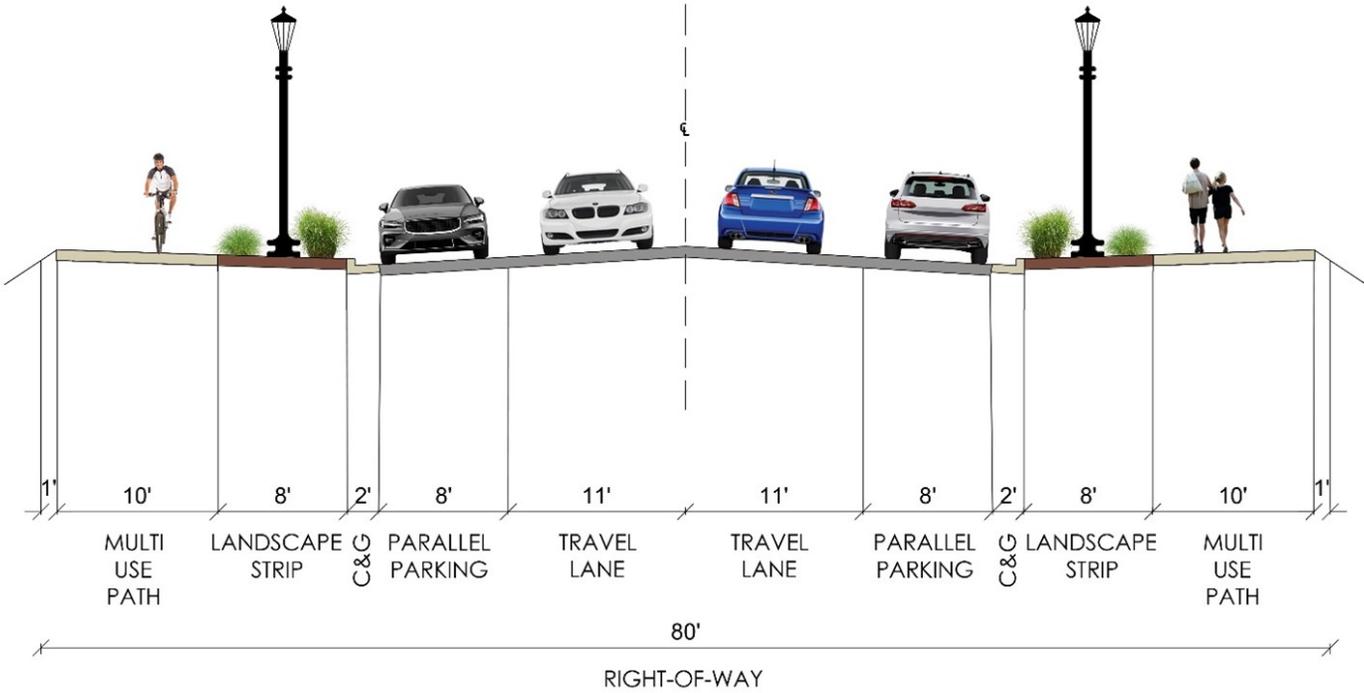
Main Street

Main St.

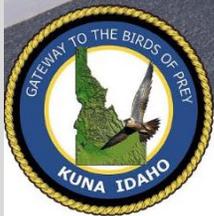


4th Street

4th Street from School Ave to Linder Ave



Rendering of 4th Street Enhanced Pedestrian Crossing

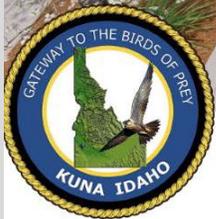


4TH STREET REVITALIZATION

City of Kuna, Idaho
Linder Avenue to School Avenue



Rendering of 4th Street looking west



4TH STREET REVITALIZATION

City of Kuna, Idaho
Linder Avenue to School Avenue



Rendering of 4th Street looking east



4TH STREET REVITALIZATION

City of Kuna, Idaho
Linder Avenue to School Avenue



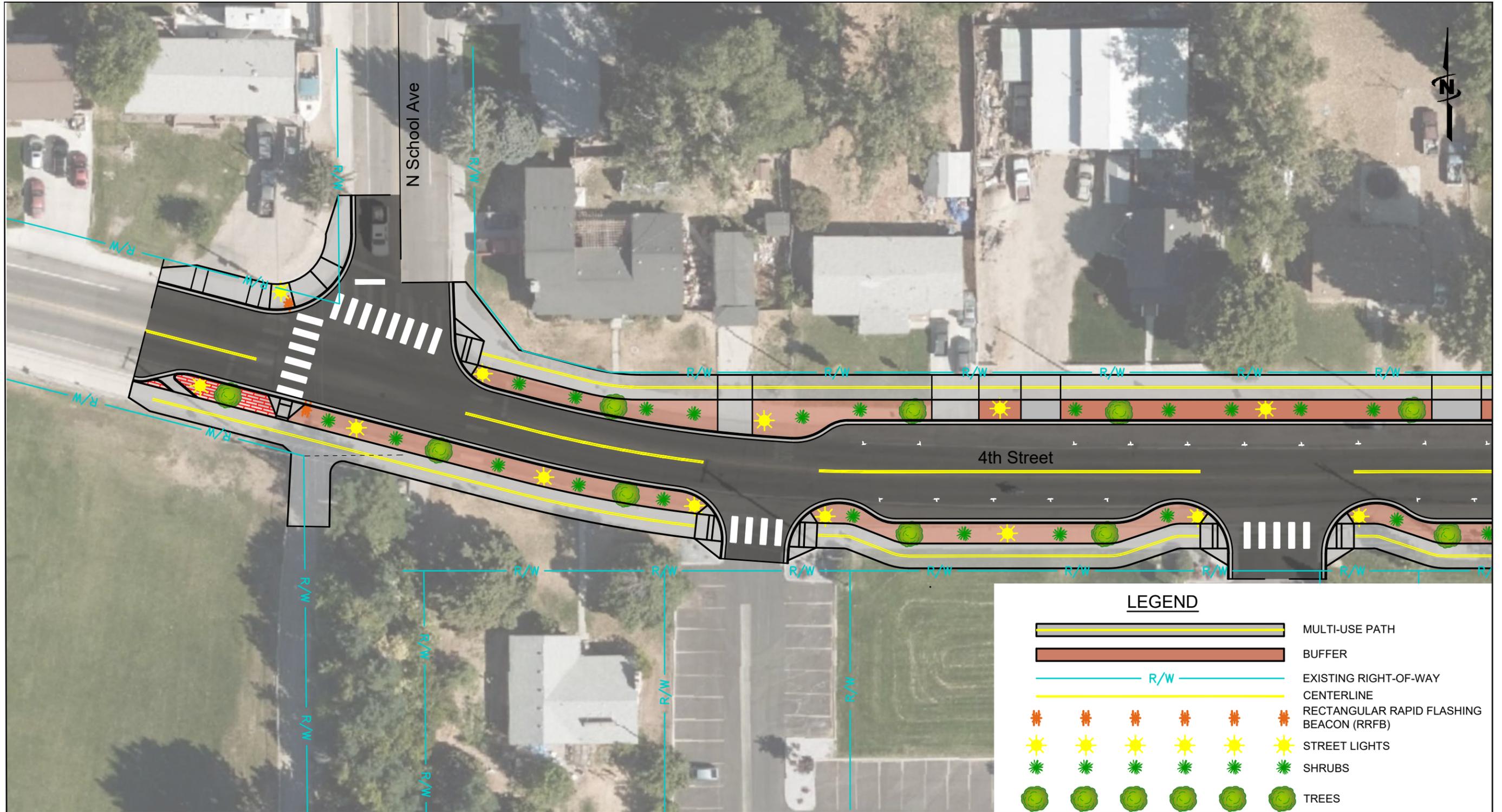
Rendering displaying the corner of Franklin Ave. and 4th Street



4TH STREET REVITALIZATION

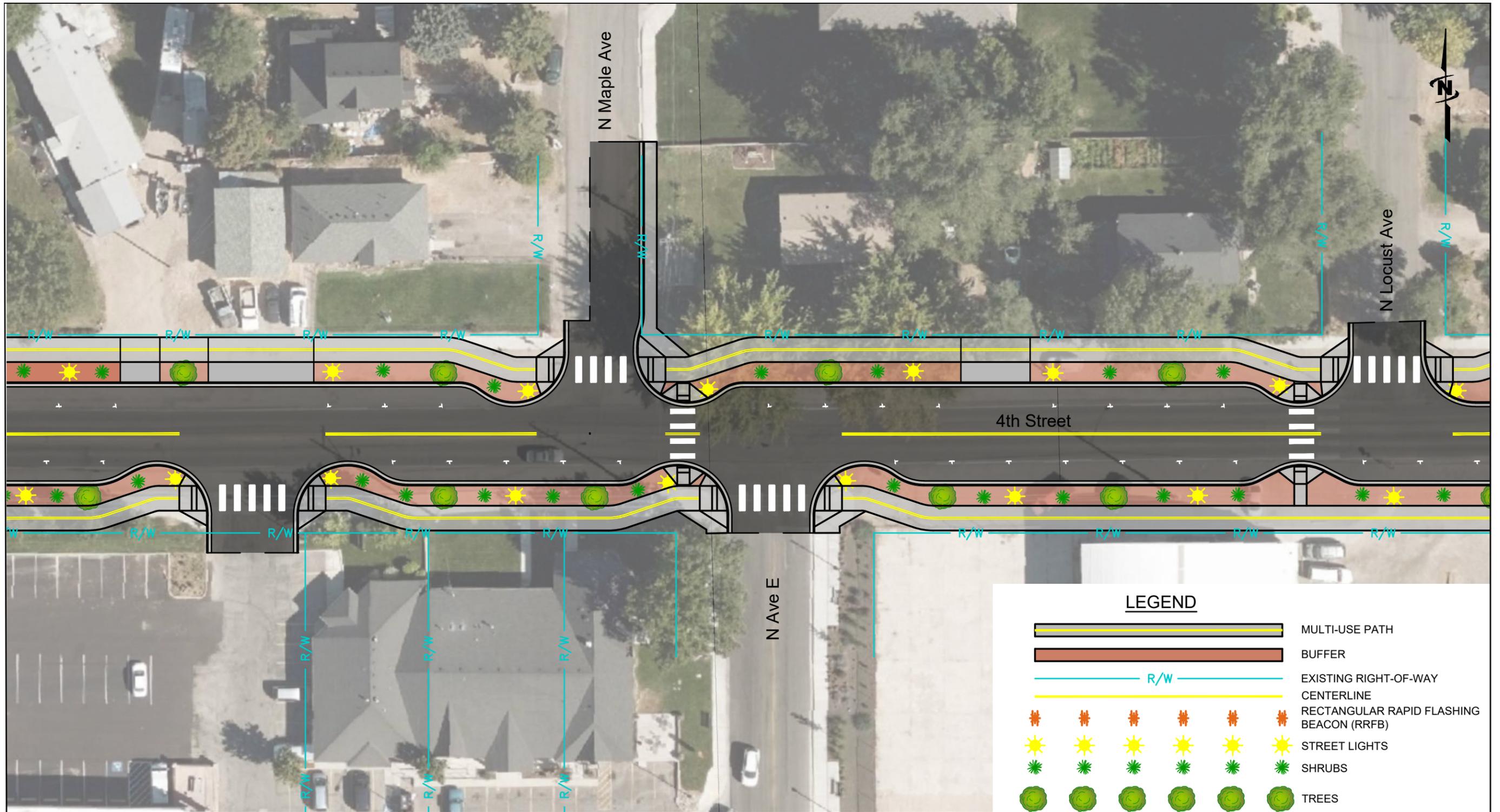
City of Kuna, Idaho
Linder Avenue to School Avenue





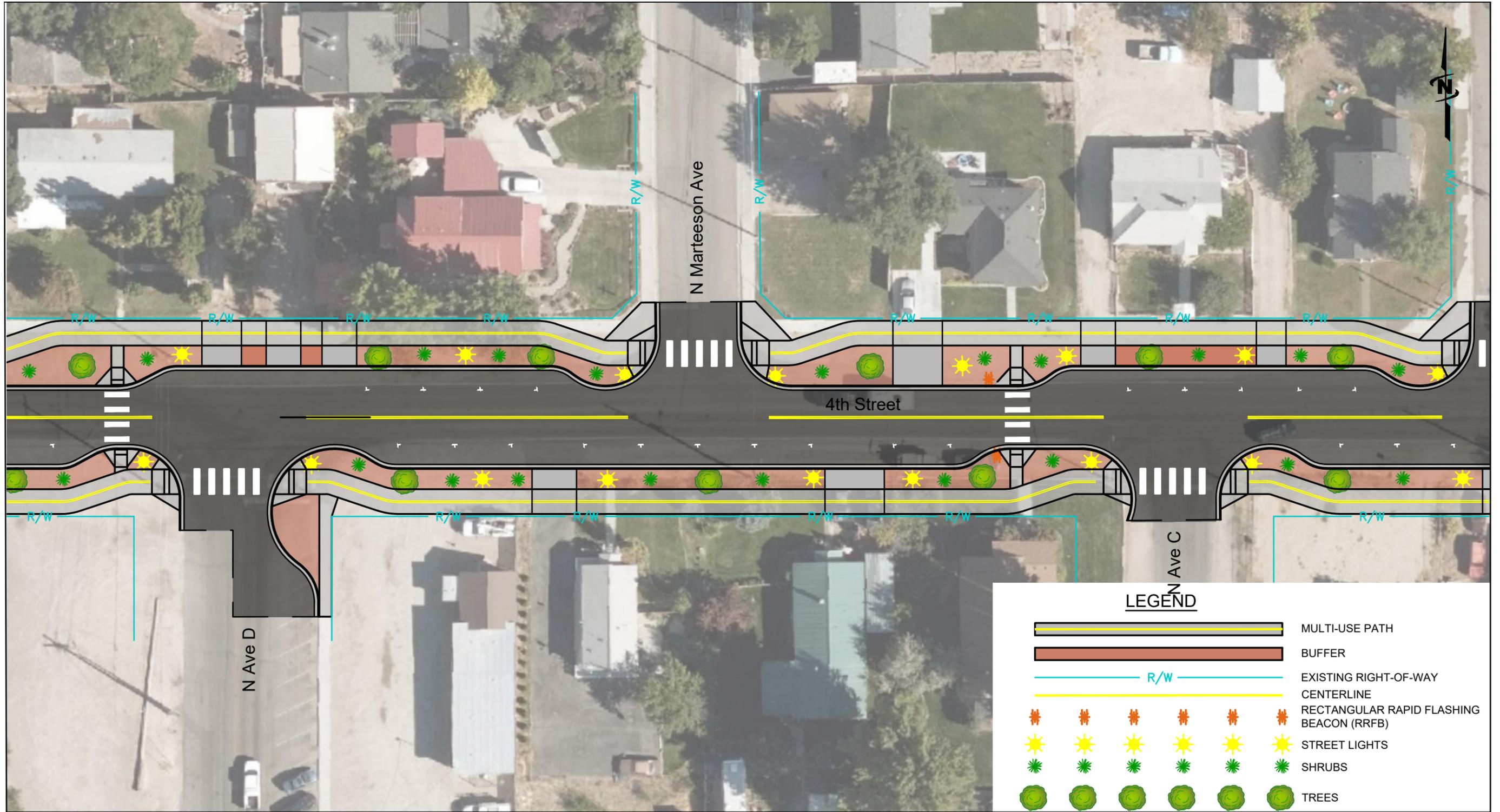
**4th Street Revitalization
City of Kuna, Idaho
Linder Avenue to School Avenue**





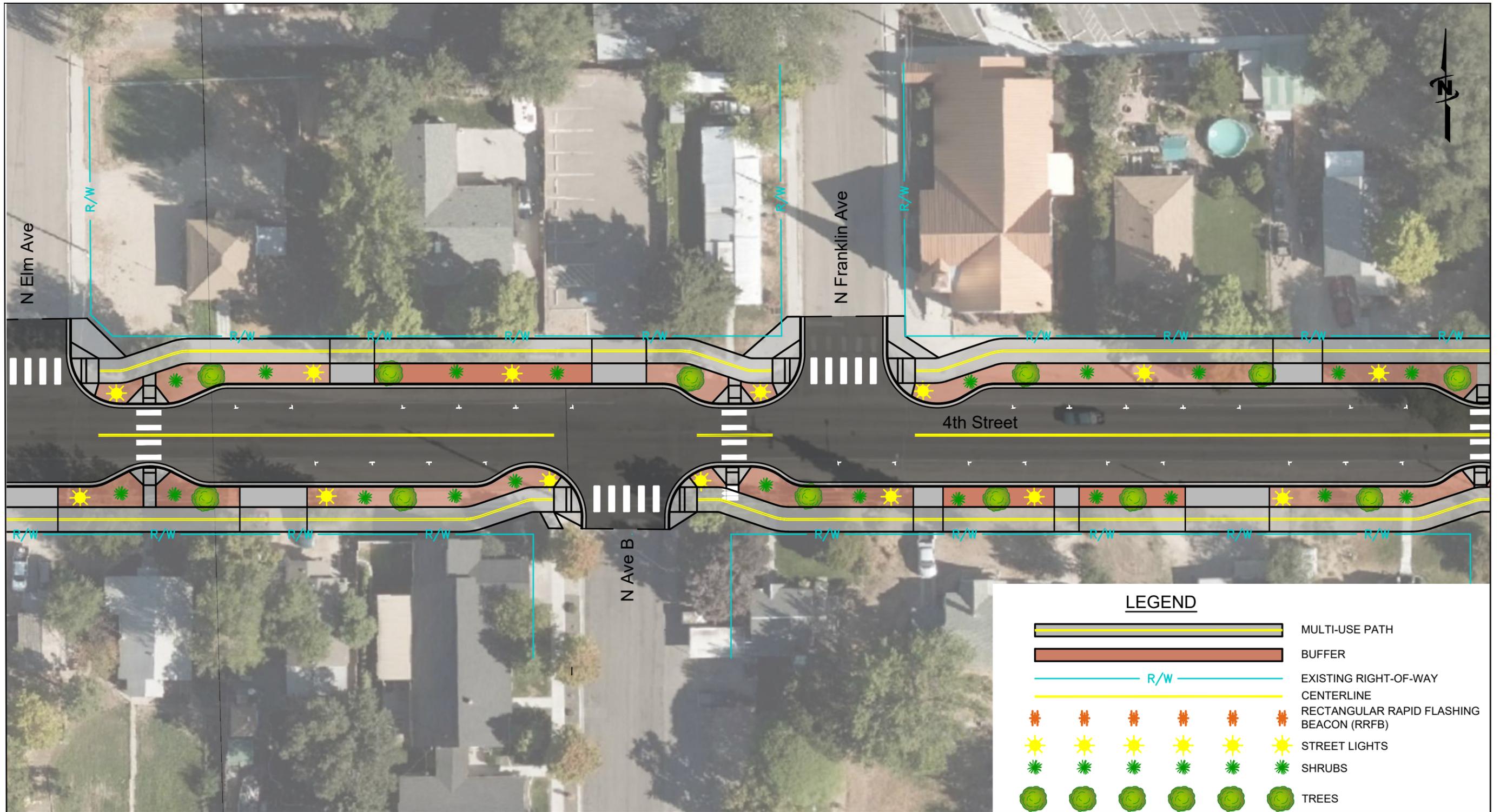
**4th Street Revitalization
City of Kuna, Idaho
Linder Avenue to School Avenue**





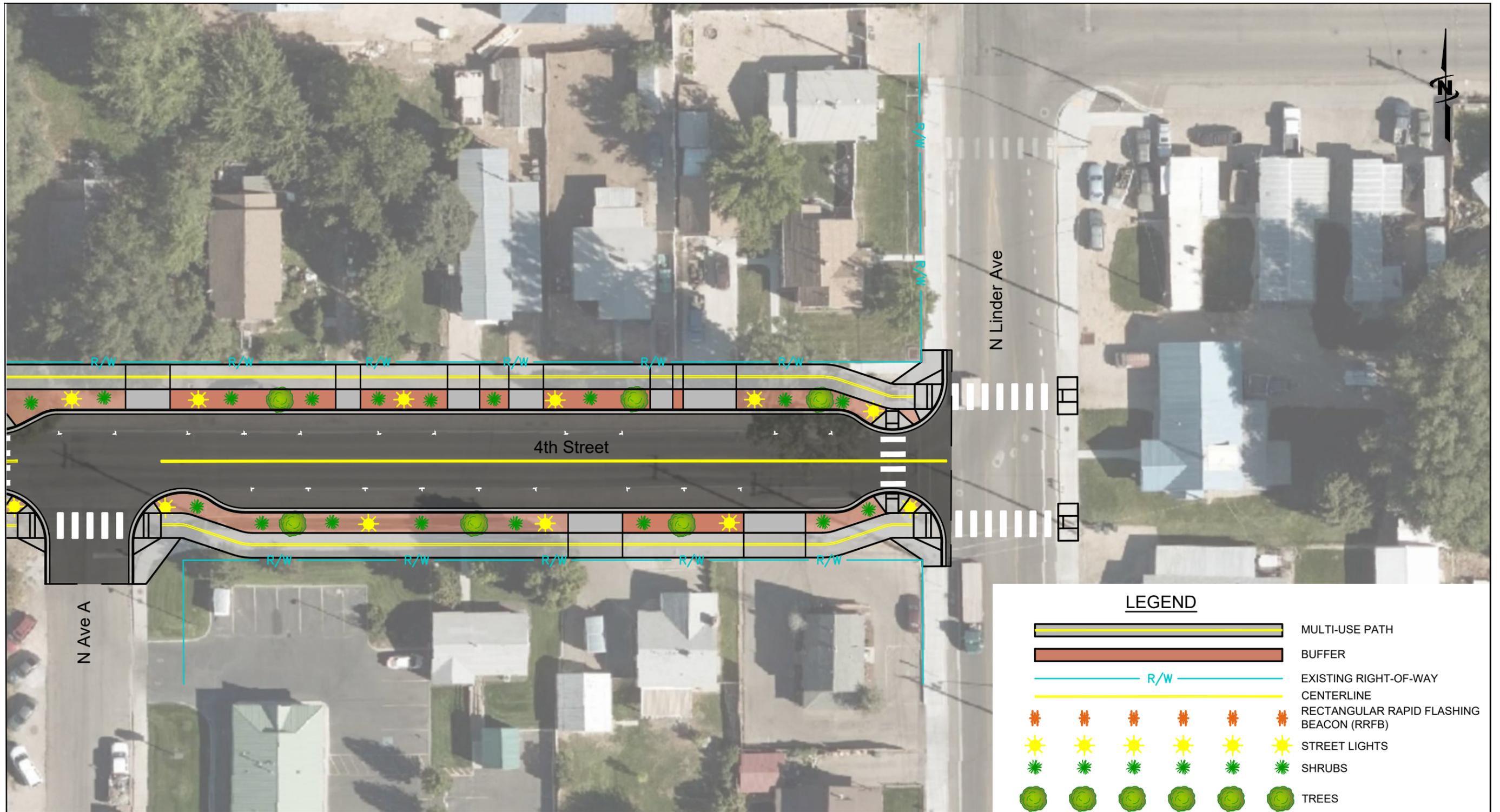
**4th Street Revitalization
City of Kuna, Idaho
Linder Avenue to School Avenue**





**4th Street Revitalization
City of Kuna, Idaho
Linder Avenue to School Avenue**





LEGEND

	MULTI-USE PATH
	BUFFER
	EXISTING RIGHT-OF-WAY
	CENTERLINE
	RECTANGULAR RAPID FLASHING BEACON (RRFB)
	STREET LIGHTS
	SHRUBS
	TREES



**4th Street Revitalization
City of Kuna, Idaho
Linder Avenue to School Avenue**





STREET LIGHTING

Downtown Kuna light fixtures are highly decorative and pedestrian friendly.

Brand

Beacon

Color

Black

Pole

14', RSA-B-F Series, Round Straight Aluminum Fluted

Base

Centennial AB

Banner Arms

5" Mounting Collar

Luminaire Fixture

Win 40 (LED), 40" Windsor

Specs attached

Hanging Flower Pots/Baskets

To be approved by the City of Kuna

Banners

Festive

Holiday

Welcoming

Artistic

RSA-B-F SERIES POLES ROUND STRAIGHT ALUMINUM

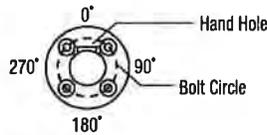
Cat.# RSA-B-F-14-50-B-TN4-BBT-GFI



Job
Kuna Street Lighting

Type

Approvals



APPLICATIONS

- Lighting installations for side and top mounting of luminaires with effective projected area (EPA) not exceeding maximum allowable loading of the specified pole in its installed geographic location

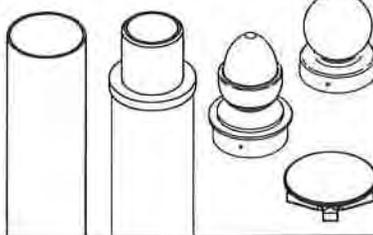
CONSTRUCTION

- **SHAFT:** One-piece straight aluminum with fluted cross section; Extruded shafts of 6061-T6 aluminum in 3/16" or 1/4" thickness. Base plate of 356 cast aluminum.
- **BOLT COVERS:** Four (4) individual bolt covers provided, painted to match pole and base finish.
- **POLE CAP OR FINALS:** Cap or decorative finials available for side mounted luminaires. Open top or tenons provided for post top mounted luminaires.
- **HAND HOLE:** Aluminum hand hole frame; Mounting provisions for grounding lug located behind cover
- **ANCHOR BOLTS:** Four galvanized anchor bolts provided per pole with minimum yield of 55,000 psi (ASTM F1554). Galvanized hardware with two washers and two nuts per bolt for leveling

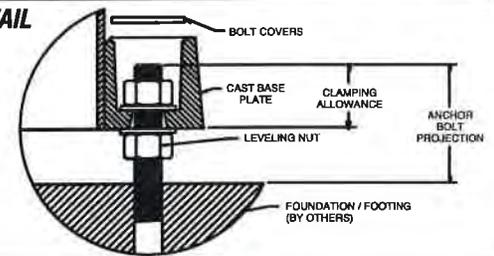
FINISH

- Durable thermoset polyester powder coat paint finish with nominal 3.0 mil thickness
- Powder paint finish coat available in twelve standard colors; Custom colors available; RAL number preferable.

TENONS & POLE CAPS



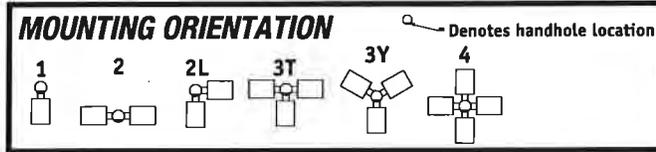
BASE DETAIL



ORDERING EXAMPLE:

RSA-B-S - **16** - **40** - **A/B/C** - **2L** - **B3** - **DBT** - **VM2**

SERIES	HEIGHT	SHAFT	THICKNESS	MOUNTING	DRILL PATTERN	FINISH	OPTIONS
RSA-B-F Round Straight Aluminum Pole Beacon Fluted	Reference page 2 Ordering matrix	Reference page 2 Ordering matrix	Reference page 2 Ordering matrix	1 Single arm mount 2 Two fixtures at 180° 2L Two fixtures at 90° 3T Three fixtures at 90° 3Y Three at 120° 4 Four fixtures at 90° OT Open top (includes pole cap) TN3 Tenon 3 x 3 TN4 Tenon 3 x 4 TN5 Tenon 4 x 5 TN8 Tenon 4 x 8 ARC Acorn Finial BAL Ball Finial	B1 Cruiser B3 VP-L B4 VP-S	BBT Basic Black Textured BMT Black Matte Textured BZT Bronze Textured DBT Dark Bronze Textured DPS Dark Platinum Smooth GNT Green Textured GYS Gray Smooth MBT Metallic Bronze Textured MST Metallic Silver Textured MTT Metallic Titanium Textured OWI Old World Iron WHT White Textured CC Custom Color- RAL Color#	GFI 20 Amp GFCI Receptacle and Cover EHH Extra Handhole C05 .5" Coupling C07 .75" Coupling C20 2" Coupling VM2 2nd mode vibration damper LAB Less Anchor Bolts



ACCESSORIES- Order Separately

Catalog Number	Description
VM2SXX	2nd mode vibration damper

1 Specify option location using logic found on page 2 (Option Orientation)



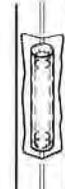
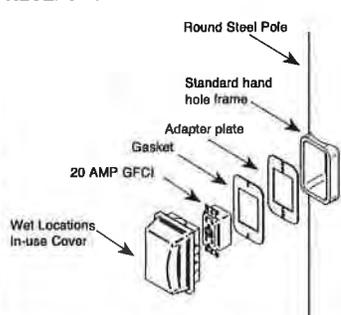
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ORDERING INFORMATION Cont.

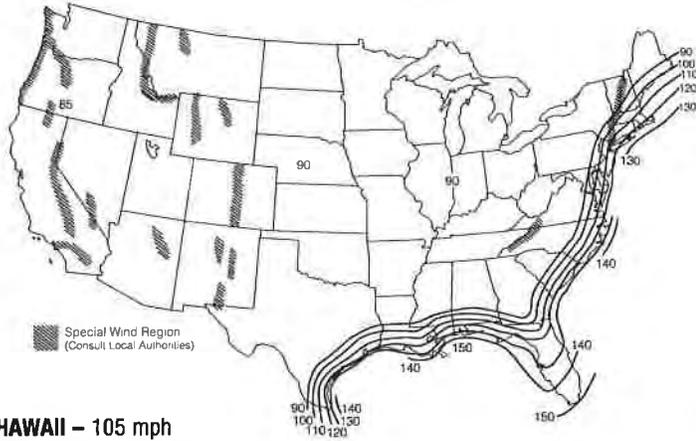
Catalog Number	Height		Nominal Shaft Dimensions	Wall Thickness	Bolt Circle (suggested)	Bolt Square	Base Plate Size	Anchor Bolt Size	Bolt Projection	Pole weight (lbs)
	Feet	Meters								
RSA-B-F-08-40-B	8	2.4	4" Fluted	.188	6.75"	4.77	9.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	28
RSA-B-F-10-40-B	10	3.0	4" Fluted	.188	6.75"	4.77	9.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	34
RSA-B-F-12-40-B	12	3.7	4" Fluted	.188	6.75"	4.77	9.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	39
RSA-B-F-14-40-B	14	4.3	4" Fluted	.188	6.75"	4.77	9.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	45
RSA-B-F-16-40-B	16	4.9	4" Fluted	.188	6.75"	4.77	9.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	51
RSA-B-F-18-40-B	18	5.5	4" Fluted	.188	6.75"	4.77	9.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	57
RSA-B-F-20-40-B	20	6.1	4" Fluted	.188	6.75"	4.77	9.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	63
RSA-B-F-22-40-B	22	6.7	4" Fluted	.188	6.75"	4.77	9.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	69
RSA-B-F-24-40-B	24	7.3	4" Fluted	.188	6.75"	4.77	9.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	74
RSA-B-F-08-50-B	8	2.4	5" Fluted	.188	7.75"	5.48"	10.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	34
RSA-B-F-10-50-B	10	3.0	5" Fluted	.188	7.75"	5.48"	10.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	41
RSA-B-F-12-50-B	12	3.7	5" Fluted	.188	7.75"	5.48"	10.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	48
RSA-B-F-14-50-B	14	4.3	5" Fluted	.188	7.75"	5.48"	10.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	55
RSA-B-F-16-50-B	16	4.9	5" Fluted	.188	7.75"	5.48"	10.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	62
RSA-B-F-18-50-B	18	5.5	5" Fluted	.188	7.75"	5.48"	10.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	69
RSA-B-F-20-50-B	20	6.1	5" Fluted	.188	7.75"	5.48"	10.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	76
RSA-B-F-22-50-B	22	6.7	5" Fluted	.188	7.75"	5.48"	10.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	83
RSA-B-F-24-50-B	24	7.3	5" Fluted	.188	7.75"	5.48"	10.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	90
RSA-B-F-12-60-C	12	3.7	6" Fluted	.25	8.75"	6.19"	11.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	72
RSA-B-F-14-60-C	14	4.3	6" Fluted	.25	8.75"	6.19"	11.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	83
RSA-B-F-16-60-C	16	4.9	6" Fluted	.25	8.75"	6.19"	11.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	94
RSA-B-F-18-60-C	18	5.5	6" Fluted	.25	8.75"	6.19"	11.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	105
RSA-B-F-20-60-C	20	6.1	6" Fluted	.25	8.75"	6.19"	11.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	116
RSA-B-F-22-60-C	22	6.7	6" Fluted	.25	8.75"	6.19"	11.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	127
RSA-B-F-24-60-C	24	7.3	6" Fluted	.25	8.75"	6.19"	11.62" Dia x 1.88" Thk	3/4 x 30 x 3	3-3/4"	138

NOTE Factory supplied template must be used when setting anchor bolts. Hubbell Lighting will deny any claim for incorrect anchorage placement resulting from failure to use factory supplied template and anchor bolts.

<p>EHH - EXTRA HANDHOLE</p>  <p>Provision for Grounding</p>	<p>C05 - C07 - C20 - COUPLING</p>  <p>2" - 11.5 NPSM Threads 3/4" - 14 NPSM Threads 1/2" - 14 NPSM Threads</p>	<p>VM2 - VIBRATION DAMPER 2ND MODE</p>  <p>Factory installed, internal damper designed to alter pole resonance to reduce movement and material fatigue caused by 2nd mode vibration.</p>	<p>GFI - 20 AMP GFCI RECEPTACLE & COVER</p>  <p>Round Steel Pole Standard hand hole frame Adapter plate Gasket 20 AMP GFCI Wet Locations In-use Cover</p>
<p>OPTION ORIENTATION</p> <p>Follow the logic below when ordering location specific options. For each option, include its orientation (in degrees) and its height (in feet). Example: Option C07 should be ordered as: RSA-B-F-16-40-B-TN3-DBT (.5" coupling on the handhole/arm side of pole, 15 feet up from the pole base) 1' spacing required between option. Consult factory for other configurations.</p>		<p>VM2SXX - VIBRATION DAMPER 2ND MODE</p> <p>Field installed, Internal damper designed to alter pole resonance to reduce movement and material fatigue caused by 2nd mode vibration.</p> <p>VM2S08 - 8' VM2S12 - 12' VM2S16 - 16' VM2S20 - 20' VM2S24 - 24'</p>	

For more information about pole vibration and vibration dampers, please consult http://cdn.spauldinglighting.com/content/products/literature/literature_files/Pole_Wind_Induced_Flyer_HL010022.pdf
Due to our continued efforts to improve our products, product specifications are subject to change without notice.

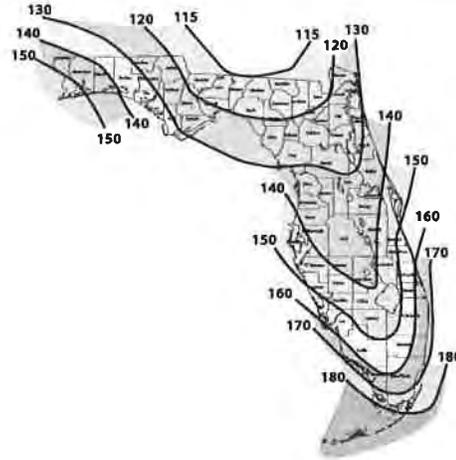
ASCE7-05 WIND MAP



HAWAII – 105 mph
PUERTO RICO – 145 mph

ASCE 7-05 wind map EPA Load Rating - 3 second gust wind speeds										
Catalog Number	85	90	100	105	110	120	130	140	145	150
RSA-B-F-08-40-B	18.0	16.0	12.8	11.5	10.4	8.6	7.1	5.9	5.4	5.0
RSA-B-F-10-40-B	13.6	12.0	9.5	8.5	7.6	6.1	4.9	4.0	3.6	3.2
RSA-B-F-12-40-B	10.5	9.2	7.1	6.3	5.6	4.3	3.4	2.6	2.3	2.0
RSA-B-F-14-40-B	8.2	7.1	5.4	4.6	4.0	3.0	2.2	1.5	1.2	1.0
RSA-B-F-16-40-B	6.4	5.5	3.9	3.3	2.8	1.9	1.1	0.6	NR	NR
RSA-B-F-18-40-B	4.9	4.0	2.7	2.2	1.7	0.9	NR	NR	NR	NR
RSA-B-F-20-40-B	3.6	2.8	1.7	1.2	0.8	NR	NR	NR	NR	NR
RSA-B-F-22-40-B	2.5	1.8	0.8	NR	NR	NR	NR	NR	NR	NR
RSA-B-F-24-40-B	1.5	0.9	NR	NR	NR	NR	NR	NR	NR	NR
RSA-B-F-08-50-B	25.0	25.0	21.2	19.2	17.3	14.3	12.0	10.1	9.3	8.6
RSA-B-F-10-50-B	22.7	20.2	16.1	14.4	13.0	10.6	8.7	7.2	6.5	6.0
RSA-B-F-12-50-B	17.9	15.8	12.4	11.1	9.9	7.9	6.3	5.1	4.5	4.0
RSA-B-F-14-50-B	14.4	12.6	9.7	8.5	7.5	5.8	4.5	3.4	3.0	2.5
RSA-B-F-16-50-B	11.6	10.0	7.5	6.5	5.6	4.2	3.0	2.1	1.7	1.3
RSA-B-F-18-50-B	9.2	7.9	5.7	4.8	4.0	2.7	1.7	0.9	0.5	NR
RSA-B-F-20-50-B	7.2	6.0	4.1	3.3	2.6	1.5	0.6	NR	NR	NR
RSA-B-F-22-50-B	5.5	4.4	2.7	2.0	1.4	NR	NR	NR	NR	NR
RSA-B-F-24-50-B	4.0	3.1	1.6	0.9	NR	NR	NR	NR	NR	NR
RSA-B-F-12-60-C	25.0	25.0	24.2	21.6	19.4	15.7	12.9	10.6	9.6	8.7
RSA-B-F-14-60-C	25.0	25.0	19.7	17.4	15.5	12.4	9.9	7.9	7.1	6.4
RSA-B-F-16-60-C	24.1	20.9	16.1	14.1	12.4	9.7	7.5	5.8	5.1	4.4
RSA-B-F-18-60-C	20.0	17.2	13.0	11.3	9.8	7.4	5.5	4.0	3.3	2.8
RSA-B-F-20-60-C	16.5	14.1	10.3	8.8	7.5	5.3	3.7	2.4	1.8	1.3
RSA-B-F-22-60-C	13.5	11.4	8.0	6.7	5.5	3.6	2.1	1.0	0.5	NR
RSA-B-F-24-60-C	11.0	9.1	6.1	4.9	3.8	2.1	0.8	NR	NR	NR

FLORIDA REGION WIND MAP



• Florida region wind map above is based upon 3-second gust winds and the 2014 Florida Building Code

Florida Building Code 2014 EPA Load Rating - 3 second gust wind speeds								
Catalog Number	115	120	130	140	150	160	170	180
RSA-B-F-08-40-B	13.5	12.3	10.4	8.8	7.5	6.4	5.5	4.8
RSA-B-F-10-40-B	10.1	9.1	7.6	6.3	5.3	4.4	3.7	3.1
RSA-B-F-12-40-B	7.6	6.9	5.6	4.5	3.7	2.9	2.4	1.9
RSA-B-F-14-40-B	5.8	5.1	4.0	3.1	2.4	1.8	1.3	0.9
RSA-B-F-16-40-B	4.3	3.7	2.7	2.0	1.3	0.8	NR	NR
RSA-B-F-18-40-B	3.0	2.5	1.7	1.0	NR	NR	NR	NR
RSA-B-F-20-40-B	1.9	1.5	0.7	NR	NR	NR	NR	NR
RSA-B-F-22-40-B	1.0	0.6	NR	NR	NR	NR	NR	NR
RSA-B-F-08-50-B	22.3	20.4	17.3	14.7	12.6	10.9	9.5	8.3
RSA-B-F-10-50-B	17.0	15.5	12.9	10.9	9.2	7.8	6.7	5.7
RSA-B-F-12-50-B	13.2	12.0	9.9	8.2	6.8	5.6	4.7	3.9
RSA-B-F-14-50-B	10.4	9.3	7.5	6.1	4.9	3.9	3.1	2.4
RSA-B-F-16-50-B	8.0	7.1	5.6	4.3	3.3	2.5	1.7	1.1
RSA-B-F-18-50-B	6.1	5.3	3.9	2.8	2.0	1.2	0.6	NR
RSA-B-F-20-50-B	4.4	3.7	2.5	1.6	0.8	NR	NR	NR
RSA-B-F-22-50-B	3.0	2.4	1.4	NR	NR	NR	NR	NR
RSA-B-F-24-50-B	1.8	1.3	NR	NR	NR	NR	NR	NR
RSA-B-F-12-60-C	25.0	24.7	20.7	17.5	14.9	12.7	10.9	9.4
RSA-B-F-14-60-C	22.1	20.1	16.6	13.9	11.7	9.8	8.2	6.9
RSA-B-F-16-60-C	18.0	16.2	13.3	10.9	9.0	7.4	6.0	4.9
RSA-B-F-18-60-C	14.5	12.9	10.4	8.3	6.7	5.3	4.1	3.1
RSA-B-F-20-60-C	11.6	10.3	8.0	6.2	4.8	3.5	2.5	1.6
RSA-B-F-22-60-C	9.2	8.0	6.1	4.4	3.1	2.0	1.1	NR
RSA-B-F-24-60-C	7.2	6.1	4.3	2.9	1.7	0.7	NR	NR



NOTES

Wind-speed Website disclaimer:

Hubbell Lighting has no connection to the linked website and makes no representations as to its accuracy. While the information presented on this third-party website provides a useful starting point for analyzing wind conditions, Hubbell Lighting has not verified any of the information on this third party website and assumes no responsibility or liability for its accuracy. The material presented in the windspeed website should not be used or relied upon for any specific application without competent examination and verification of its accuracy, suitability and applicability by engineers or other licensed professionals. Hubbell Lighting Inc. does not intend that the use of this information replace the sound judgment of such competent professionals, having experience and knowledge in the field of practice, nor to substitute for the standard of care required of such professionals in interpreting and applying the results of the windspeed report provided by this website. Users of the information from this third party website assume all liability arising from such use. Use of the output of these referenced websites do not imply approval by the governing building code bodies responsible for building code approval and interpretation for the building site described by latitude/longitude location in the windspeed report. <http://windspeed.atcouncil.org>

- Allowable EPA, to determine max pole loading weight, multiply allowable EPA by 30 lbs.
- The tables for allowable pole EPA are based on the ASCE 7-05 Wind Map or the Florida Region Wind Map for the 2010 Florida Building Code. The Wind Maps are intended only as a general guide and cannot be used in conjunction with other maps. Always consult local authorities to determine maximum wind velocities, gusting and unique wind conditions for each specific application
- Allowable pole EPA for jobsite wind conditions must be equal to or greater than the total EPA for fixtures, arms, and accessories to be assembled to the pole. Responsibility lies with the specifier for correct pole selection. Installation of poles without luminaires or attachment of any unauthorized accessories to poles is discouraged and shall void the manufacturer's warranty
- Wind speeds and listed EPAs are for ground mounted installations. Poles mounted on structures (such as bridges and buildings) must consider vibration and coefficient of height factors beyond this general guide; Consult local and federal standards
- Wind Induced Vibration brought on by steady, unidirectional winds and other unpredictable aerodynamic forces are not included in wind velocity ratings. Consult Hubbell Lighting's Pole Vibration Application Guide for environmental risk factors and design considerations. http://cdn.spauldinglighting.com/content/products/literature/literature_files/Pole_Wind_Induced_Flyer_HL010022.pdf
- Extreme Wind Events like, Hurricanes, Typhoons, Cyclones, or Tornadoes may expose poles to flying debris, wind shear or other detrimental effects not included in wind velocity ratings

Due to our continued efforts to improve our products, product specifications are subject to change without notice.



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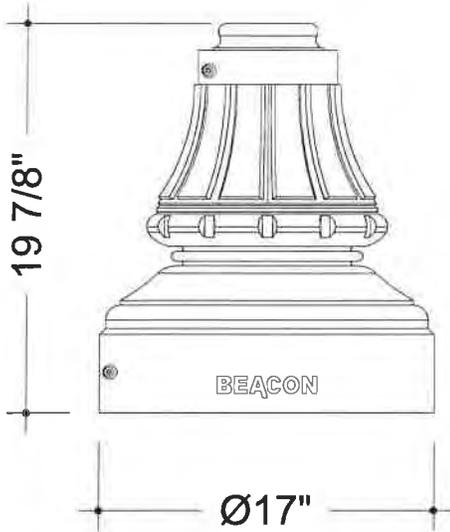


HUBBELL
Lighting

RSA-B-F POLES-SPEC 7/16

Sample **CEAB** **5** **BBT**
Ordering **CEAB** / **5** / **BBT**
 A **B** **C**

DETAILS



A. MODEL

CEAB Centennial AB ✓

B. POST SHAFT DIAMETER

3 3"
4 4"
5 5" ✓

C. COLOR

BBT basic black textured ✓
BMT black matte textured
WHT white textured
MBT metallic bronze textured
BZT bronze textured
DBT dark
GYS gray smooth
DPS dark platinum smooth
GNT green textured
MST metallic silver textured
MTT metallic titanium textured
OWI old world iron
RAL _____

Construction: All cast aluminum parts shall be low copper alloy A356. All extruded aluminum parts shall be alloy 6061-T6, 6063-T5 or equal. Standard fluted shaft profile shall be 16-flat flutes for 3"OD and 12-flat flutes for 4", 5", & 6"OD.

Base Covers: require specification of smooth or fluted shafts of the size required to meet wind load requirements

Vibration Dampeners: Vibration dampener pads shall be provided when required by customer or deemed necessary by Beacon Products. Please consult factory for bridge mounted applications.

EPA (effective projected area): EPA is defined as (projected surface area X drag factor) and measured in ft². Allowable post, luminaire arm, luminaire and accessory EPAs are derived from the most current published AASHTO (American Association of State Highway and Transportation Officials) standard, currently AASHTO 2001 (50yr design life). Customer assumes all responsibility for selecting the appropriate post for installation (consult factory for assistance). Luminaire arm, luminaire and accessory EPA must be equal to or less than allowable EPA of post. Consult a professional engineer for compliance with local codes and standards.

Anchor Bolt: Anchor bolts, sized as required, double hex nuts and flat washers shall be hot dipped galvanized steel. A bolt circle template shall be provided for installation.

Fasteners: All fasteners shall be Corrosion Resistant. When tamper resistant fasteners are required, spanner HD (snake eye) style shall be provided (special tool required, available at additional cost).

Finish: Finish shall be a Beacote V polyester powder-coat electrostatically applied and thermocured. Beacote V finish shall consist of a five stage iron phosphate chemical pretreatment regimen with a polymer primer sealer, oven dry off, and top coated with a thermoset super TGIC polyester powder coat finish. The finish shall meet the AAMA 605.2 performance specification which includes passing a 3000 hour salt spray test for corrosion resistance and resists cracking or loss of adhesion per ASTM D522 and resists surface impacts of up to 160 inch-pound.

Limited Warranty: Beacon Products warrants its products, to the original purchaser, against defects in materials and workmanship for proper usage for a period of 5 years after date of production, when properly installed, maintained and appropriately specified. See Warranty Information on www.beaconproducts.com for complete details and exclusions.

Due to our continued efforts to improve our products, product specifications are subject to change without notice.



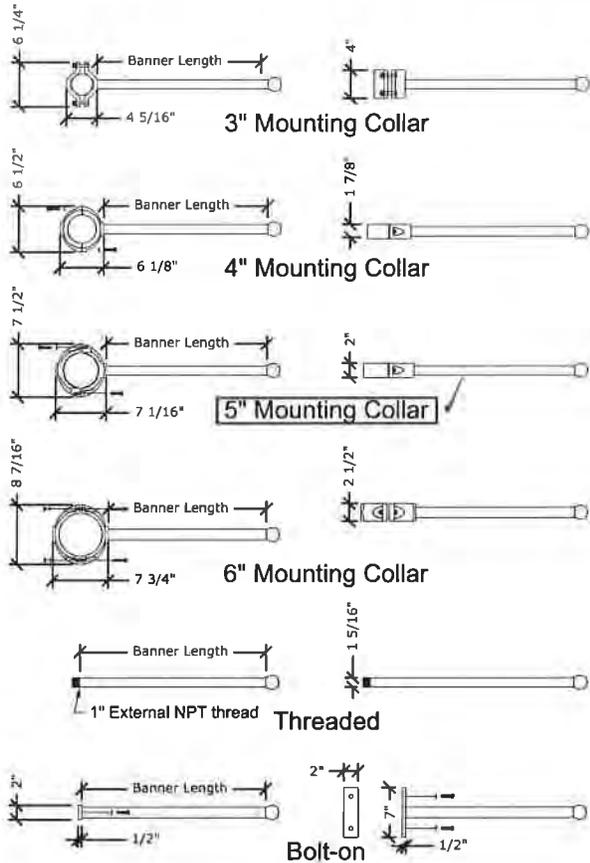
Banner Arms

Project Information

Name / Location: Kuna Street Lighting /
 Type / Quantity: /
 Sold to: _____
 PO#: _____

Approvals

Banner Arm Mounting Styles



Specifications

Construction:

All cast aluminum parts shall low copper alloy A356. All extruded aluminum parts shall be alloy 6061-T6, 6063-T5 or equal.

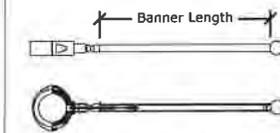
Fasteners:

All fasteners shall be stainless steel. When tamper resistant fasteners are required, spanner HD (snake eye) style shall be provided (special tool required, available at additional cost), consult factory for availability.

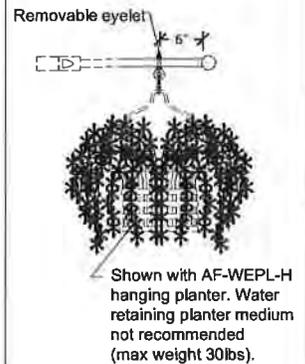
Finish:

Finish shall be Beacote III polyester powdercoat electrostatically applied and thermocured. Poles shall be subjected to chemical pre-treatment prior to painting by immersion process.

Options



Breakaway Coupler System
 breakaway speed varies with banner size
 (consult factory)



Planter Eyelet

Ordering Example:

AA-0024 / 4"MTC / BA / BKT

AA-00 26" / 5"MTC / BA / BB

Length	Mounting	Options	Finish
<input type="checkbox"/> 18 18"	<input type="checkbox"/> 3"MTC mounting collar for 3" shaft	<input type="checkbox"/> BA breakaway coupler system	<input type="checkbox"/> BB black
<input type="checkbox"/> 20 20"	<input type="checkbox"/> 4"MTC mounting collar for 4" shaft	<input type="checkbox"/> PA removable planter eyelet*	<input type="checkbox"/> BZ bronze
<input type="checkbox"/> 22 22"	<input type="checkbox"/> 5"MTC mounting collar for 5" shaft	<input type="checkbox"/> 2X two banner arms at 180°	<input type="checkbox"/> BG green
<input type="checkbox"/> 24 24"	<input type="checkbox"/> 6"MTC mounting collar for 6" shaft		<input type="checkbox"/> BW white
<input type="checkbox"/> 30 30"	<input type="checkbox"/> BO bolted connection		<input type="checkbox"/> BY gray
	<input type="checkbox"/> TO threaded connection		<input type="checkbox"/> RAL RAL-
			<input type="checkbox"/> OTHER

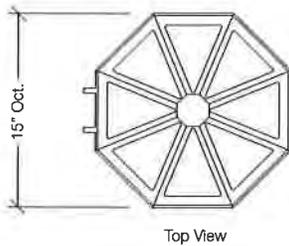
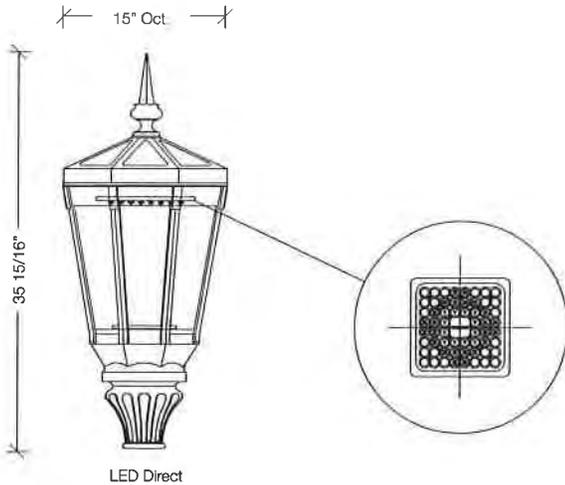
Consult factory for premium and verte finishes. Custom colors available.

Notes:

* Consult factory

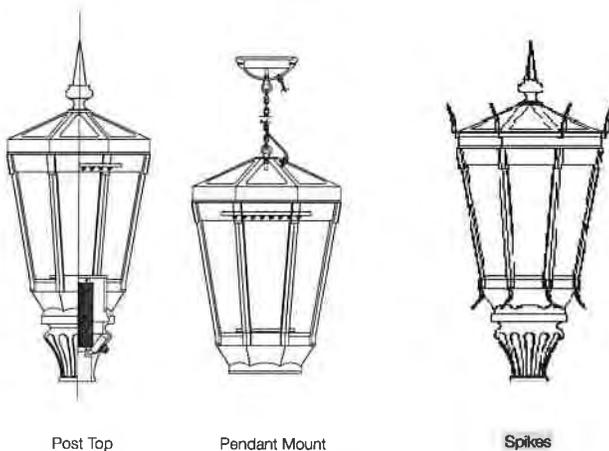
Sample	WIN40 ✓	AC	24NB-55	5K	UNV	DIR5	PEC	PT	GENXX	SPK	BBT
Ordering	WIN40 /	PF /	36NB-80	5K /	UNV /	DIR5 /	/	PT /	/	SPK /	BBT
	A	B	C	D	E	F	G	H	I	J	K

DETAILS



MOUNTING OPTIONS

STYLE OPTIONS



A. MODEL

WIN40 Windsor 40"

G. ELECTRICAL OPTIONS

PEC photocell, button

B. LENS OPTIONS

- AC** acrylic, clear
- AF** acrylic, frosted
- AS** acrylic, seeded
- AW** acrylic, white
- PC** polycarbonate, clear
- PF** polycarbonate, frosted ✓

H. CONTROL OPTIONS

- GENI-XX** energeni ¹
- SWP** sitesync wireless pre-commission^{2,4}

C. ENGINE - WATTS

- 24NB-55** 55 Watts - LED array
- 36NB-80** 80 Watts - LED array ✓
- 48NB-110** 110 Watts - LED array
- 60NB-136** 136 Watts - LED array

H. MOUNTING OPTIONS

- PT** post top ✓
- PM** pendant mount ²

J. STYLE OPTIONS

- SPK** spikes ✓

D. CCT - COLOR TEMP

- 3K** 3000K
- 4K** 4000K
- 5K** 5000K (std.) ✓

K. COLOR

- BBT** basic black textured ✓
- BMT** black matte textured
- WHT** white textured
- MBT** metallic bronze textured
- BZT** bronze textured
- DBT** dark bronze textured
- GYS** gray smooth
- DPS** dark platinum smooth
- GNT** green textured
- MST** metallic silver textured
- MTT** metallic titanium textured
- OWI** old world iron
- RAL** _____

E. VOLTAGE

- UNV** 120-277V ✓
- 347** 347V
- 480** 480V

F. OPTICS

- DIR2** type II
- DIR3** type III
- DIR4** type IV
- DIR5** type V ✓

¹ When ordering Energeni, specify the routine setting code (example GENI-04). See Energeni brochure and instructions for setting table and options. Not available with sensor options.

² consult factory

³ Must specify group and zone information at time of order. See www.hubbelling.com/controls/sitesync for further details

⁴ Not available with other control or sensor options.



SPECIFICATIONS

Intended Use:

- The Beacon Windsor luminaire is available with a choice of different LED Wattage configurations and optical distributions designed to replace HID lighting up to 250W MH or HPS.

Construction:

- The electrical chamber/fitter shall be cast aluminum.
- Decorative fitter shall be designed to accommodate the driver assembly and shall be mounted to 3" OD x 3" H tenon and be secured by three or more stainless steel set screws.

Electrical:

- 100V through 277V, 50 Hz to 60 Hz (UNV)
- Power factor is ≥ 0.90 at full load
- All electrical components are rated at 50,000 hours at full load and 25°C ambient conditions per MIL- 217F Notice 2.
- One piece optical cartridge system consisting of an LED engine, LED lamps, optics, gasket and stainless steel bezel.
- Cartridge is held together with internal brass standoffs soldered to the board so that it can be field replaced as a one piece optical system.
- Silicone gasket ensures a weather-proof seal around each individual LED.
- Fixture electrical compartment shall contain all LED driver components.
- Rated ambient operating temperature -40°C to 40°C.
- Surge protection - 20KA
- Lifeshield™ Circuit - protects luminaire from excessive temperature. The device shall activate at a specific, factory-preset temperature, and progressively reduce power over a finite temperature range. Operation shall be smooth and undetectable to the eye. Thermal circuit is designed to "fail on", allowing the luminaire to revert to full power in the event of an interruption of its power supply, or faulty wiring connection to the drivers. The device shall be able to co-exist with other 0-10V control devices (occupancy sensors, external dimmers, etc.).

Controls/Options:

- Available with Energeni for optional set dimming, timed dimming with simple delay, or timed dimming based on time of night (see www.beaconproducts.com/products/energeni).
- London can be specified with SiteSync™ wireless control system for reduction in energy and maintenance cost while optimizing light quality 24/7. See ordering information or visit www.hubbellighting.com/sitesync

Finish:

- IFS polyester powder-coat electrostatically applied and thermocured.
- IFS finish consists of a five stage pretreatment regimen with a polymer primer sealer and top coated with a thermoset super TGIC polyester powder coat finish.
- The finish meets the AAMA 605.2 performance specification which includes passing a 3000 hour salt spray test for corrosion resistance and resists cracking or loss of adhesion per ASTM D522 and resists surface impacts of up to 160 inch-pounds.

Listings:

- Listed to UL8750 , UL1598 and CSA22.2#250.13-14 for wet locations

Warranty:

- Five year limited warranty (for more information visit: www.hubbellighting.com/resources/warranty)

Accessories and Services (Ordered Separately)

Catalog Number	Description
SWUSB*	SiteSync loaded on USB flash drive (Windows based only)
SWTAB*	SiteSync Windows Tablet
SWBRG*	SiteSync Wireless Bridge Node

*When ordering with SiteSync, one of the following interface options must be chosen and ordered separately. Each option contains the SiteSync License, GUI and Bridge Node.
 + If needed, an additional Bridge Node can be ordered.

PRECOMMISSIONED SITESYNC ORDERING INFORMATION: When ordering a fixture with the SiteSync lighting control option, additional information will be required to complete the order. The SiteSync Commissioning Form or alternate schedule information must be completed. This form includes Project location, Group Information, and Operating schedules. For more detailed information please visit www.HubbellLighting.com/sitesync/ or contact Hubbell Lighting tech support at (800) 345-4928.

SiteSync fixtures with occupancy sensor (SWPM) require the mounting height of the fixture for selection of the lens.

Due to our continued efforts to improve our products, product specifications are subject to change without notice.



GREENBELT/ PATHWAY LIGHTING

Brand

Sun Valley

Color

Green

Pole

14', Smooth, Round

Base

700

Banner Arms

Field Rotatable, 18"

Luminaire fixture

LAA Series - LED

Specs attached



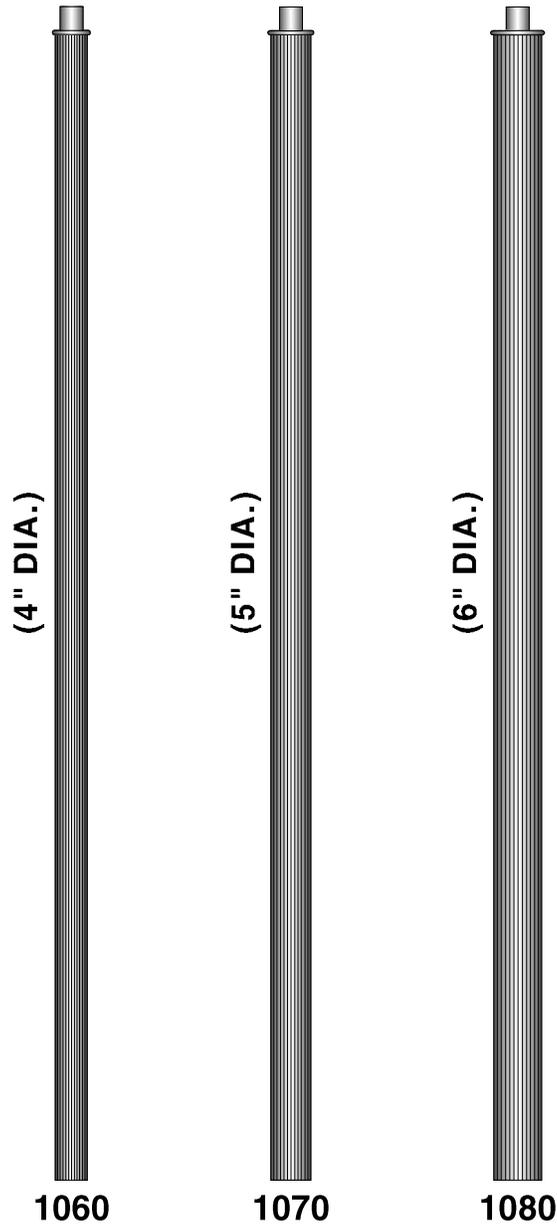
Bollard Lights

Black, 8" Round, 40" H

Specs attached

FLUTED

ROUND EXTRUDED FROM 6063 ALLOY ALUMINUM. HEAT TREATED TO PRODUCE A T6 TEMPER. SHAFTS HAVE EVENLY SPACED HIGHLY DETAILED RAISED VERTICAL FLUTES.



(FLUTED ALUMINUM SHAFTS)
SHAFT WALL THICKNESS .188

POLES PROVIDED WITH TENON TOP
POST TOP (PT): 2 7/8" O.D. X 3"
MULTIPLE LUMINAIRES: 2 7/8" O.D. X 6"



The Lighting Tradition

sun valley
LIGHTING

660 WEST AVENUE O, PALMDALE, CA 93551
(661) 233-2000
FAX (661) 233-2001
www.sunvalleylighting.com



BASE

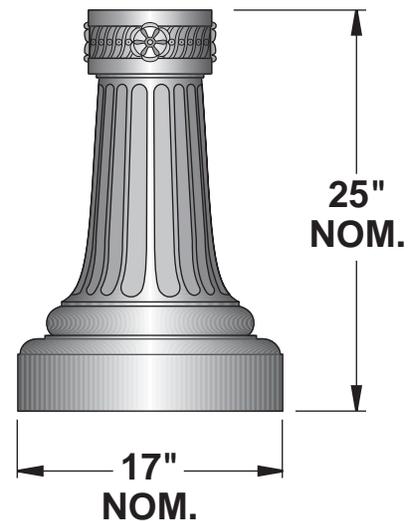
ONE PIECE CORROSION RESISTANT, DURABLE CAST ALUMINUM CONSTRUCTION. MINIMUM .210 WALL THICKNESS. BASE CONSISTS OF A SMOOTH, TAPERED BOTTOM SECTION, AND A HIGHLY DETAILED, DECORATIVE FLUTED SECTION, A DECORATIVE CAST ALUMINUM COLLAR AND A FLUTED HAND HOLE COVER CONTOURED TO BASE DESIGN. HAND HOLE COVER SUPPLIED WITH TAMPER RESISTANT HARDWARE. GROUNDING LUG PROVIDED INSIDE BASE OPPOSITE HAND HOLE.

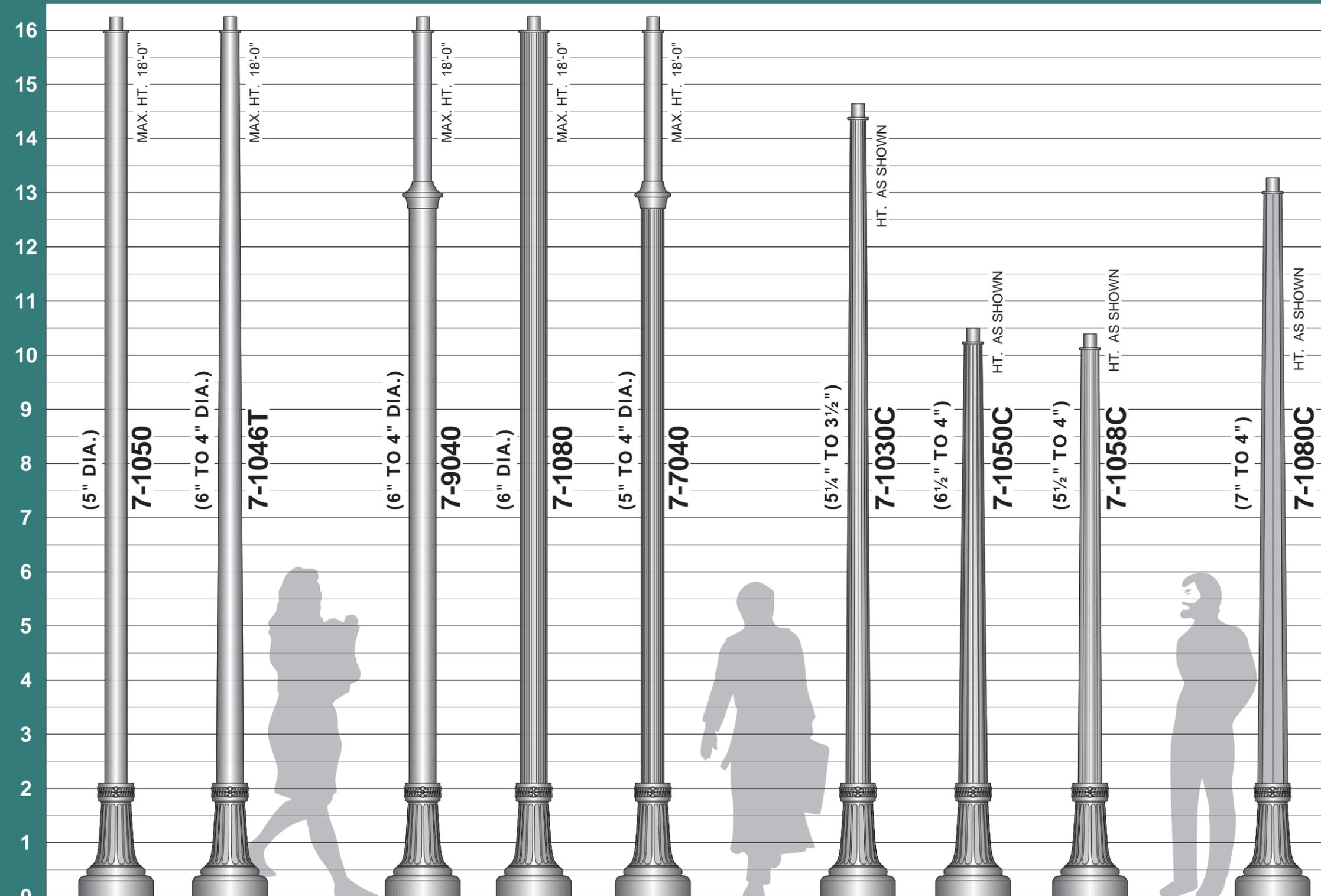
ANCHORAGE

A 3/4" CAST ALUMINUM ANCHOR RING IS WELDED 1" ABOVE BOTTOM OF BASE TO ACCEPT (4) FULLY GALVANIZED ANCHOR BOLTS. EACH BOLT SUPPLIED WITH TWO NUTS AND TWO WASHERS. ANCHORAGE IS FASTENED THROUGH HAND HOLE.

FINISH

ELECTROSTATICALLY APPLIED BAKED ON TEXTURED ACRYLIC ENAMEL. (SEE PAGE 5 FOR OPTIONAL FINISH AND COLOR SELECTION)

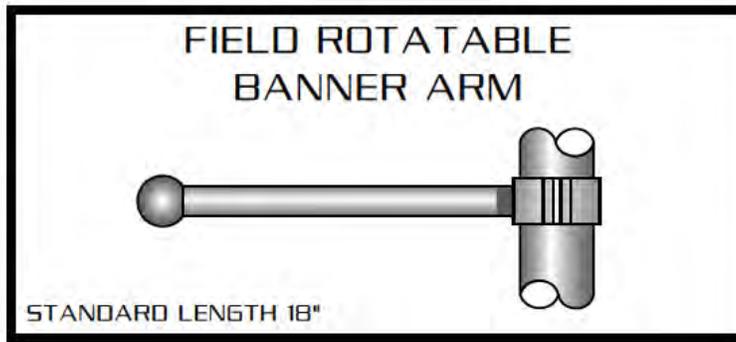




SHAFT IS CIRCUMFERENTIALLY WELDED TO BASE FOR ONE PIECE CONSTRUCTION
SEE SHAFT SECTION FOR SPECIFICATIONS AND ADDITIONAL STYLES

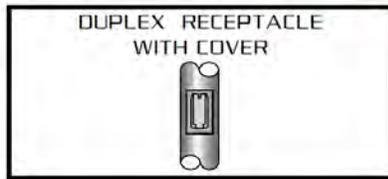
- TENONS**
- STANDARD: 2 7/8" O.D.  PT23
 - OPTIONS: 2 3/8" O.D.  PT31
 - 3 1/2" O.D. 
 - DUPLEX OR G.F.I. RECEPTACLE WITH COVER  DUP GFI
 - FIELD ROTATABLE OR FIXED BANNER ARM  RBA
 -  FBA
 - BANNER EYE BOLT  BEB
 - BREAK AWAY BANNER ARM  BBA
 - FIELD ROTATABLE OR FIXED BASKET HOLDER  RBH
 -  FBH
 - FIELD ROTATABLE STREET SIGN  RSS
 - FIELD ROTATABLE LADDER REST  RLR
 -  20"
 - FIELD ROTATABLE OR FIXED FLAG HOLDER  RFH
 -  FFH

RBA



ACCESSORIES

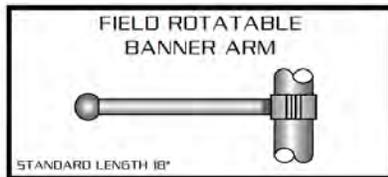
DUP



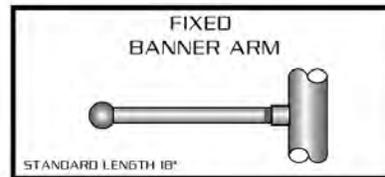
GFI



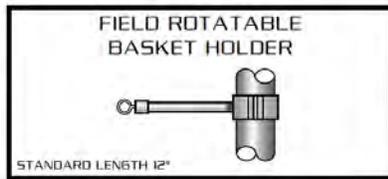
RBA



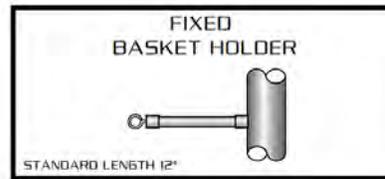
FBA



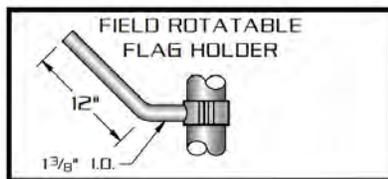
RBH



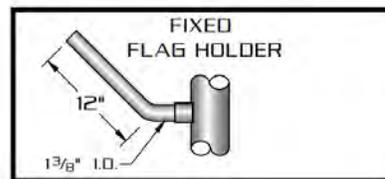
FBH



RFH



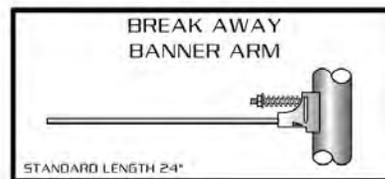
FFH



BEB



BBA



RBC



HNB



SOLID STATE AREA LIGHTING

LAA SERIES-LED

SPECIFICATIONS

LUMINAIRE

One piece high impact polycarbonate patterned diffusing globe provided with durable corrosion resistant cast aluminum fitter. Standard fitter has 3" I.D. opening for tenon. All hardware is Stainless Steel.

LED POWER ARRAY™

Three-dimensional array of individual LED Tubes fastened to a retaining plate. Each LED Tube consists of circuit board populated with a multiple of LED's and is mechanically fastened to a radial aluminum heat sink. An acrylic Lens and end cap protects the LED Tube's internal components.

ANGLED POWER ARRAY™: Micro-Reflectors mounted around each LED control the raw light output. LED Tubes are uniquely aimed horizontally and vertically and combined to produce highly efficient IES Distribution Types II, III, IV and V. Used in conjunction with a clear patterned diffusing globe.

VERTICAL POWER ARRAY™ WITH GLASS REFRACTOR: LED Tubes are aligned vertically and arranged radially to produce an even raw light distribution that simulates standard light sources. Array is secured within a 6" Prismatic Glass Refractor which provides the optical control. Used in conjunction with a clear patterned diffusing globe.

VERTICAL POWER ARRAY™: LED Tubes are aligned vertically and arranged radially to produce an even raw light distribution that simulates standard light sources. Produces a minimal glare, symmetric diffuse light distribution. Used in conjunction with an opal patterned diffusing globe.

LED EMITTERS

High Output LED's are driven at 350mA for nominal 1 Watt output each. LED's are available in standard Neutral White (4000K), or optional Cool White (5000K) or Warm White (3000K). Consult Factory for other LED options.

LED DRIVER

UL and CUL recognized High Power Factor, Constant Current LED drivers operate on input voltages from 120-277VAC, 50/60hz. Consult Factory for 347-480VAC. Driver is mechanically fastened to a retaining bracket. Main power quick disconnect provided. Driver has a minimum 4KV of internal surge protection, 10KV & 20KV Surge Protector optional. Dimming and High-Low Driver options available.

FINISH

Polyester powder coat incorporates four step iron phosphate process to pretreat metal surface for maximum adhesion. Top coat is baked at 400°F for maximum hardness and exterior durability.

PROJECT NAME: _____

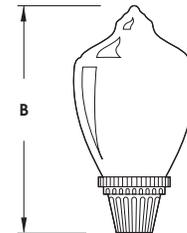
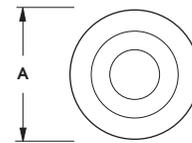
PROJECT TYPE: _____



LAA

* SHOWN WITH STANDARD -YA FITTER

PATENT PENDING



FIXTURE	A	B
LAA1*	15.5" 394mm	28.5" 724mm
LAA2*	13.5" 343mm	26" 660mm



2013140

LAA SERIES - LED

S P E C I F I C A T I O N S

OPTIONAL FITTERS



* AVAILABLE WITH LAA1 ONLY
 ** AVAILABLE WITH MINI BUTTON PHOTOCELLS
 *SEE OPTIONS

LED POWER ARRAY™ MODULES



LAA1-LED E.P.A.=1.35

ANGLED ARRAY
 Available in:
 8 Array 48 LED Max.

**GLASS REFRACTOR w/
 VERTICAL ARRAY**
 Available in:
 6 Array 36 LED Max.

VERTICAL ARRAY
 Available in:
 6 Array 48 LED Max.
 8 Array 64 LED Max.

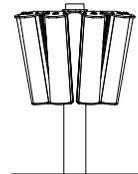


LAA2-LED E.P.A.=1.13

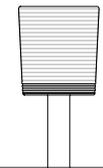
ANGLED ARRAY
 Available in:
 8 Array 48 LED Max.

**GLASS REFRACTOR w/
 VERTICAL ARRAY**
 Available in:
 6 Array 36 LED Max.

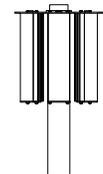
VERTICAL ARRAY
 Available in:
 6 Array 48 LED Max.
 8 Array 64 LED Max.



APA - Angled Array



**GRV - Glass Refractor
 w/ Vertical Array**



VPA - Vertical Array

(Specifications subject to
 change without prior notice.)

BRASS
 FINIAL

ORNAMENTAL
 FINIAL

PATENT PENDING

O R D E R I N G I N F O R M A T I O N

LUMINAIRE-FITTER	OPTICS	# of LED's	COLOR	VOLTAGE	MOUNTING	FINISH	OPTIONS
LUMINAIRE	OPTICS	LED			MOUNTING	FINISH	OPTIONS
LUMINAIRE-FITTER	ANGLED POWER ARRAY (CLEAR PATTERNED GLOBE)	# of LEDs	COLOR		ARM MOUNT	STANDARD TEXTURED FINISH	LENS OPTIONS:
<input type="checkbox"/> LAA1-LED-YA	<input type="checkbox"/> APA - II	LAA1	<input type="checkbox"/> NW (4000K)* *STANDARD	<input type="checkbox"/> 1	<input type="checkbox"/> BLACK RAL-9005-T	<input type="checkbox"/> CLEAR PATTERNED POLYCARBONATE ... CP (STANDARD GLOBE)	
<input type="checkbox"/> LAA1-LED-YB	<input type="checkbox"/> APA - III	LAA2	<input type="checkbox"/> CW (5000K)	<input type="checkbox"/> 2-180	<input type="checkbox"/> WHITE RAL-9003-T	<input type="checkbox"/> OPAL PATTERNED POLYCARBONATE ... WP	
<input type="checkbox"/> LAA1-LED-YC	<input type="checkbox"/> APA - IV	<input type="checkbox"/> 48LED (53 Watts)	<input type="checkbox"/> WW (3000K)	<input type="checkbox"/> 2-90	<input type="checkbox"/> GREY RAL-7004-T	<input type="checkbox"/> CLEAR PATTERNED ACRYLIC (LAA1) ... CA	
<input type="checkbox"/> LAA1-LED-YD	<input type="checkbox"/> APA - V	<input type="checkbox"/> 32LED (36 Watts)	<input type="checkbox"/> OTHER LED COLORS AVAILABLE CONSULT FACTORY	<input type="checkbox"/> 3-90	<input type="checkbox"/> DARK BRONZE RAL-8019-T	<input type="checkbox"/> BRASS FINIAL ... BF	
<input type="checkbox"/> LAA1-LED-YE		<input type="checkbox"/> 36LED (40 Watts)	VOLTAGE	<input type="checkbox"/> 3-120	<input type="checkbox"/> GREEN RAL-6005-T	<input type="checkbox"/> ORNAMENTAL FINIAL ... OF	
<input type="checkbox"/> LAA1-LED-YN	GLASS REFRACTOR (CLEAR PATTERNED GLOBE)	LAA1	<input type="checkbox"/> 120	<input type="checkbox"/> 4-90		<input type="checkbox"/> HOUSE SIDE SHIELD ... HS	
<input type="checkbox"/> LAA1-LED-YO	<input type="checkbox"/> GRV - III	LAA2	<input type="checkbox"/> 208	<input type="checkbox"/> 3-120		<input type="checkbox"/> DIMMABLE DRIVER(S) (0-10V) ... DIM	
<input type="checkbox"/> LAA1-LED-YP	<input type="checkbox"/> GRV - V	<input type="checkbox"/> 36LED (40 Watts)	<input type="checkbox"/> 240	<input type="checkbox"/> 4-90		<input type="checkbox"/> HIGH-LOW DIMMING FOR HARDWIRED SWITCHING OR NON-INTEGRATED MOTION SENSOR ... HLSW	
<input type="checkbox"/> LAA2-LED-YA	VERTICAL POWER ARRAY (OPAL GLOBE)	LAA1	<input type="checkbox"/> 277	<input type="checkbox"/> 3-120		<input type="checkbox"/> PHOTO CELL + VOLTAGE (EXAMPLE: PC120V) ... PC+V	
<input type="checkbox"/> LAA2-LED-YB	<input type="checkbox"/> VPA - SYM	LAA2	<input type="checkbox"/> 347	<input type="checkbox"/> 4-90		<input type="checkbox"/> 10KV SURGE PROTECTOR ... 10SP	
<input type="checkbox"/> LAA2-LED-YC		<input type="checkbox"/> 64LED (71 Watts)	<input type="checkbox"/> 480	<input type="checkbox"/> WM		<input type="checkbox"/> 20KV SURGE PROTECTOR (277V & 480V Only) ... 20SP	
<input type="checkbox"/> LAA2-LED-YD		<input type="checkbox"/> 48LED (53 Watts)		<input type="checkbox"/> PT			
<input type="checkbox"/> LAA2-LED-YE		<input type="checkbox"/> 36LED (40 Watts)					



KBR8 LED

LED Specification Bollard



Catalog Number
Notes
Type

Hit the Tab key or mouse over the page to see all interactive elements.

Introduction

The KBR8 Bollard is a stylish, fully integrated LED solution for walkways. It features a sleek, modern design and is carefully engineered to provide long-lasting, energy-efficient lighting with a variety of optical and control options for customized performance.

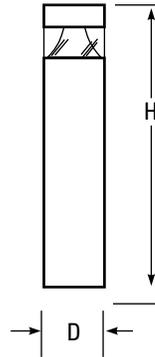
With an expected service life of over 20 years of nighttime use and up to 70% in energy savings over comparable 100W metal halide luminaires, the KBR8 Bollard is a reliable, low-maintenance lighting solution that produces sites that are exceptionally illuminated.

Specifications

8" Round
(20.3 cm)

Height: 40"
(101.6 cm)

Weight (max): 27 lbs
(12.25 kg)



Ordering Information

EXAMPLE: KBR8 LED 16C 700 40K SYM MVOLT DDBXD

KBR8 LED												
Series	LEDs	Drive current		Color temperature		Distribution		Voltage	Control options	Other options	Finish <i>(required)</i>	
KBR8 LED	Asymmetric 12C 12 LEDs ¹	350	350 mA	30K	3000 K	ASY	Asymmetric ¹	MVOLT ⁵	Shipped installed PE Photoelectric cell, button type DMG 0-10v dimming wires pulled outside fixture (for use with an external control, ordered separately) ELCW Emergency battery backup, CA Title 20 Noncompliant ⁶	Shipped installed SF Single fuse (120, 277, 347V) ^{4,7} DF Double fuse (208, 240V) ^{4,7} H24 24" overall height H30 30" overall height H36 36" overall height FG Ground-fault festoon outlet L/AB Without anchor bolts (3 bolt base) L/AB4 4 bolt retrofit base without anchor bolts ⁸	DWHXD	White
		450	450 mA ^{3,4}	40K	4000 K			120 ⁵			DNAXD	Natural aluminum
		530	530 mA	50K	5000 K	208 ⁵	DDBXD	Dark bronze				
	700	700 mA	AMBPC	Amber phosphor converted	240 ⁵	DBLXD	Black					
	Symmetric 16C 16 LEDs ²			AMBLW	Amber limited wavelength ^{3,4}			277 ⁵		DDBTXD	Textured dark bronze	
					347 ⁴	DBLBXD	Textured black					
										L/AB	DNATXD	Textured natural aluminum
										L/AB4	DWHGXD	Textured white

Accessories

Ordered and shipped separately.

MRAB U Anchor bolts for KBR8 LED³

NOTES

- Only available in the 12C, ASY version.
- Only available in the 16C, SYM version.
- Only available with 450 AMBLW version.
- Not available with ELCW.
- MVOLT driver operates on any line voltage from 120-277V (50/60 Hz). Specify 120, 208, 240 or 277 options only when ordering with fusing (SF, DF options), or photocontrol (PE option).
- Not available with 347V. Not available with fusing. Not available with 450 AMBLW.
- Single fuse (SF) requires 120, 277, or 347 voltage option. Double fuse (DF) requires 208 or 240 voltage option.
- MRAB U not available with L/AB4 option.



Performance Data

All values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts. Actual performance may differ as a result of end-user environment and application. Actual wattage may differ by +/- 8% when operating between 120-480V +/- 10%.

Light Engines	Drive Current	System Watts	3000 K					4000 K					5000 K					Limited Wavelength Amber					
			Lumens	LPW	B	U	G	Lumens	LPW	B	U	G	Lumens	LPW	B	U	G	Lumens	LPW	B	U	G	
Asymmetric 3 Engines (12 LEDs)	350	16	641	40	1	1	1	809	51	1	1	1	870	54	1	1	1						
	530	22	947	43	1	1	1	1,191	54	1	1	1	1,282	58	1	1	1						
	700	31	1,214	40	1	1	1	1,527	51	1	1	1	1,646	55	1	1	1						
	Amber 450	16																324	20	0	1	0	
Symmetric 4 Engines (16 LEDs)	350	20	888	44	1	0	0	1,116	56	1	0	0	1,203	60	1	0	0						
	530	28	1,254	45	1	0	0	1,598	57	1	0	1	1,719	61	1	0	1						
	700	39	1,608	41	1	0	1	2,022	52	1	0	1	2,180	56	2	0	1						
	Amber 450	20																374	19	0	0	0	

Note: Available with phosphor-converted amber LED's (nomenclature AMBPC). These LED's produce light with 97+% >530 nm. Output can be calculated by applying a 0.7 factor to 4000 K lumen values and photometric files.

Projected LED Lumen Maintenance

Data references the extrapolated performance projections for the platforms noted in a **25°C ambient**, based on 10,000 hours of LED testing (tested per IESNA LM-80-08 and projected per IESNA TM-21-11).

To calculate LLF, use the lumen maintenance factor that corresponds to the desired number of operating hours below. For other lumen maintenance values, contact factory.

Operating Hours	0	25,000	50,000	100,000
Lumen Maintenance Factor	1.00	0.98	0.97	0.95

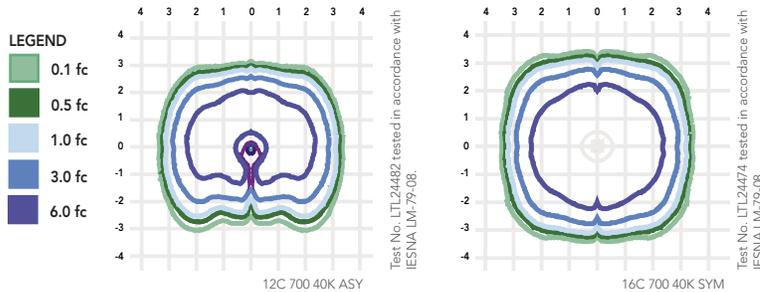
Electrical Load

Light Engines	Drive Current (mA)	System Watts	Current (A)				
			120	208	240	277	347
12C	350	16W	0.158	0.118	0.114	0.109	0.105
	530	22W	0.217	0.146	0.136	0.128	0.118
	700	31W	0.296	0.185	0.168	0.153	0.139
	Amber 450	16W	0.161	0.120	0.115	0.110	0.106
16C	350	20W	0.197	0.137	0.128	0.121	0.114
	530	28W	0.282	0.178	0.162	0.148	0.135
	700	39W	0.385	0.231	0.207	0.185	0.163
	Amber 450	20W	0.199	0.139	0.130	0.123	0.116

Photometric Diagrams

To see complete photometric reports or download .ies files for this product, visit Lithonia Lighting's [KBR8 Bollard homepage](#).

Isofootcandle plots for the KB LED Bollards. Distances are in units of mounting height (3').



FEATURES & SPECIFICATIONS

INTENDED USE

The rugged construction and clean lines of the KBA bollard is ideal for illuminating building entryways, walking paths, and pedestrian plazas, as well as any other location requiring a low mounting height light source with fully cutoff illumination.

CONSTRUCTION

One-piece 8-inch round extruded aluminum shaft with thick side walls for extreme durability, a high-impact clear acrylic lens and welded top cap. Die-cast aluminum mounting ring allows for easy leveling even in sloped locations and a full 360-degree rotation for precise alignment during installation. Three ½" x 11" anchor bolts with double nuts and washers and 3 ¾" bolt circle template ensure stability. Overall height is 42" standard.

FINISH

Exterior parts are protected by a zinc-infused super durable TGIC thermoset powder coat finish that provides superior resistance to corrosion and weathering for maximum retention of gloss and luster. A tightly controlled multi-stage process ensures a minimum 3-mil thickness for a finish that can withstand the elements without cracking or peeling. Available in both textured and non-textured finishes.

OPTICS

Two fully cutoff optical distributions are available: symmetrical and asymmetrical. IP66 sealed LED light engine provides smoothly graduated illumination without any uplight. Light engines are available in standard 4000 K (>70 CRI) or optional 3000 K (>80 CRI) or 5000 K (67 CRI). Limited-wavelength amber LEDs are also available.

ELECTRICAL

Light engines consist of high-efficacy LEDs mounted to metal-core circuit boards to maximize heat dissipation and promote long life (L95/100,000 hours at 700mA at 25°C). Class 2 electronic drivers are designed for an expected life of 100,000 hours with < 1% failure rate. Electrical components are mounted on a removable power tray.

LISTINGS

CSA certified to U.S. and Canadian standards. Light engines are IP66 rated. Rated for -40°C minimum ambient. Cold-weather emergency battery backup rated for -20°C minimum ambient.

BUY AMERICAN

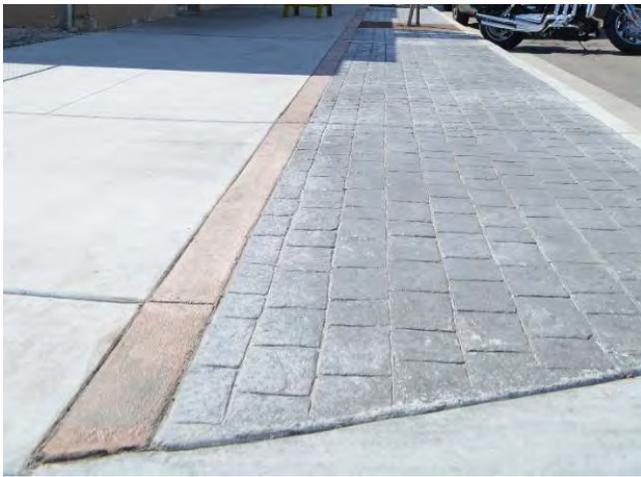
This product is assembled in the USA and meets the Buy America(n) government procurement requirements under FARS, DFARS and DOT. Please refer to www.acuitybrands.com/resources/buy-american for additional information.

WARRANTY

Five-year limited warranty. Complete warranty terms located at www.acuitybrands.com/CustomerResources/Terms_and_conditions.aspx.

Note: Actual performance may differ as a result of end-user environment and application. All values are design or typical values, measured under laboratory conditions at 25 °C. Specifications subject to change without notice.





SIDEWALKS

Tree grates

Urban Accessories Kiva 2-Piece/5-ft square

Tree placement

20' to 40' apart

Avoid obstructing signage

Concrete Colors

Red: Weathered Oak BASF RHEOCOLOR RC2505

Gray: Stone Harbor BASF RHEOCOLOR RC2501 15 lbs/CY of concrete

Stamped Concrete Pattern

Asher Cut Slate

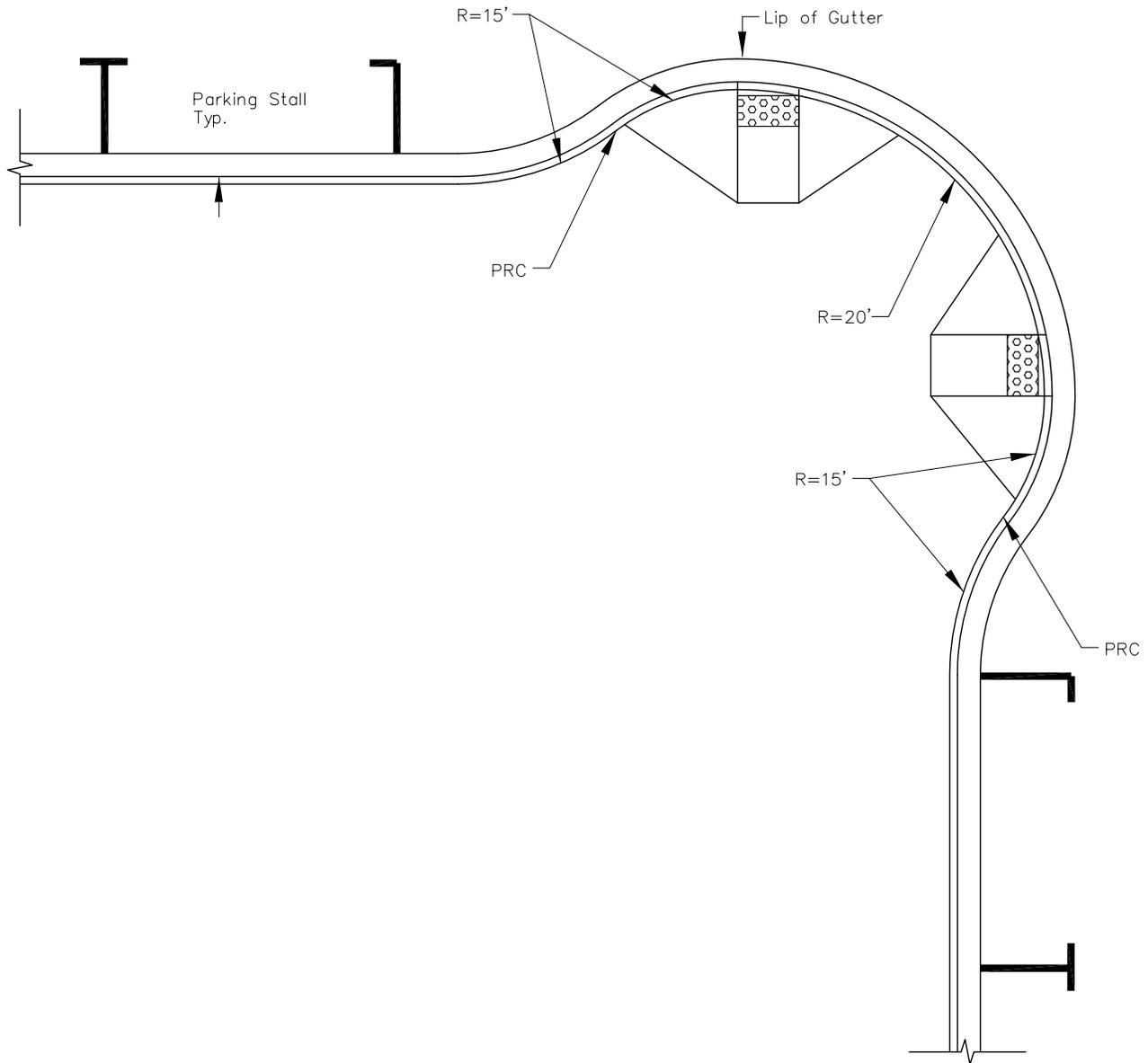
Bulbouts

ACHD SD-710F

City of Kuna Colored Concrete pattern

Specs attached

ACHD Bulbout Standard



NOTES:

- ① NO TANGENT SECTION BETWEEN REVERSE CURVES IS REQUIRED, BUT ALLOWED IF NEEDED
- ② THIS DETAIL IS THE MINIMUM RADII ALLOWED FOR REVERSED CURVES AT CURB BULBOUTS, RADII GREATER THAN THE 15' MINIMUM IS ALLOWED IF REQUESTED AND APPROVED BY ACHD.

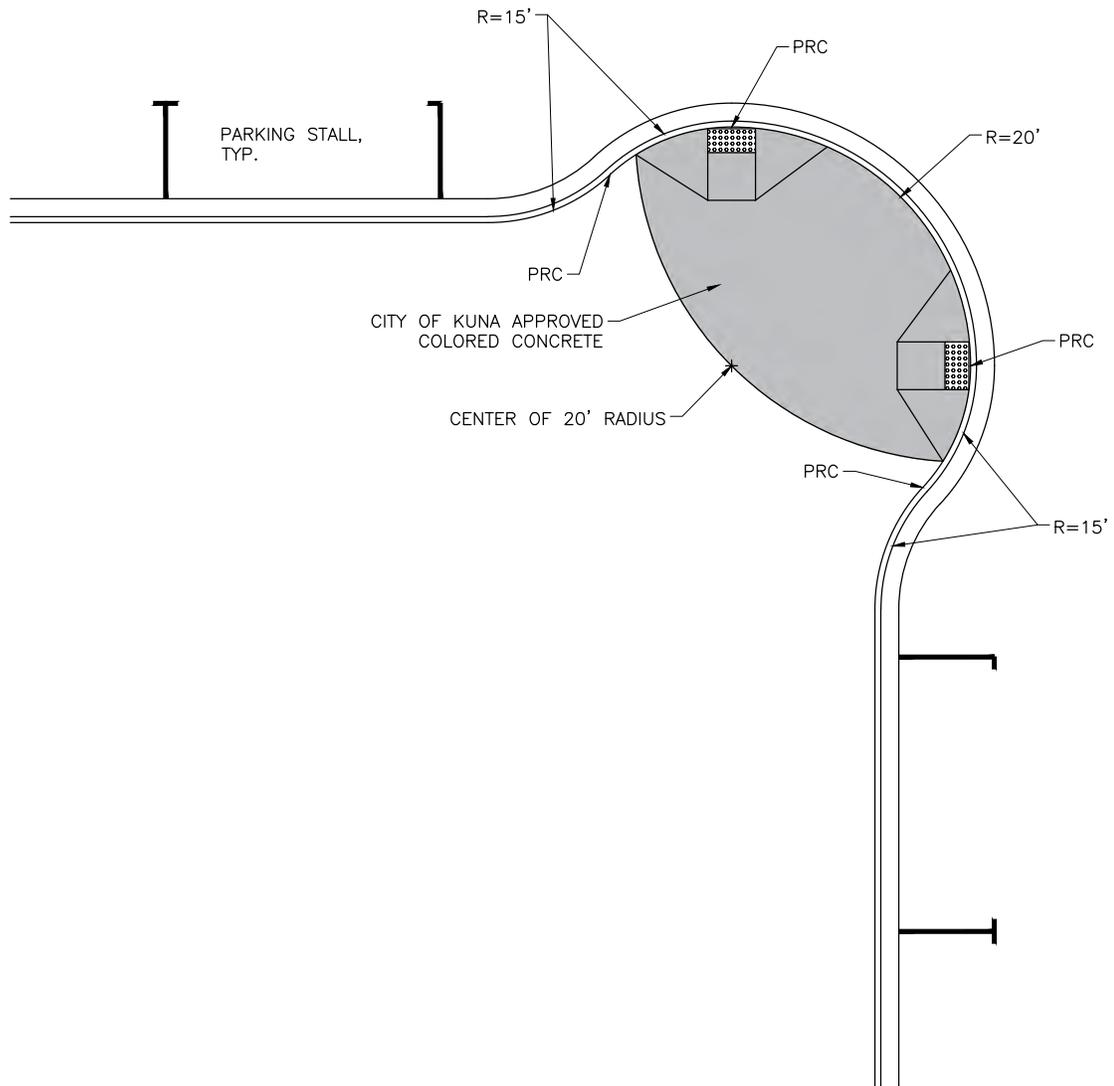
2017 ACHD REVISION

IDAHO STANDARDS
FOR PUBLIC WORKS
CONSTRUCTION
(ACHD SUPPLEMENT)

MINIMUM CURB RADII FOR
CURB BULBOUTS

STANDARD DRAWING
NO. SD-710F

Kuna Main Street Bulbout Standard



NOTES:

1. NO TANGENT SECTION BETWEEN REVERSE CURVES IS REQUIRE, BUT ALLOWED IF NEEDED.
2. THIS DETAIL IS THE MINIMUM RADII ALLOWED FOR REVERSE CURVES AT CURB BULBOUTS. RADII GREATER THAN THE 15' MINIMUM IS ALLOWED IF REQUESTED AND APPROVED BY THE CITY OF KUNA AND ACHD.



STREET FURNITURE

Downtown Kuna Street Furniture is decorative, artistic, functional, and accessible to increase visual interest and improve storefront experience.

Items provided by the city of Kuna

Benches

Trash Receptacles

Bike Racks

Bike Stations

Water Fountains

Items provided by merchant or property owner

Black, neutral or in accordance with color palettes:

Railing/fencing

Seating

Planters

Art

To be approved by the Kuna Arts Commission

DOWNTOWN DESIGN STANDARDS AMENDMENTS

Resolution No. R40-2024 (July 2, 2024)	A RESOLUTION OF THE CITY COUNCIL OF KUNA, IDAHO: <ul style="list-style-type: none">• MAKING CERTAIN FINDINGS AND DECLARATION OF AUTHORITY; AND• ADOPTING THE “CITY OF KUNA DOWNTOWN DESIGN STANDARDS”; AND• DIRECTING THE CITY CLERK; AND• PROVIDING AN EFFECTIVE DATE.
Resolution No. R18-2025 (February 18, 2025)	A RESOLUTION OF THE CITY COUNCIL OF KUNA, IDAHO: <ul style="list-style-type: none">• MAKING CERTAIN FINDINGS AND DECLARATION OF AUTHORITY; AND• AMENDING THE “CITY OF KUNA DOWNTOWN DESIGN STANDARDS”; AND• DIRECTING THE CITY CLERK; AND• PROVIDING AN EFFECTIVE DATE.